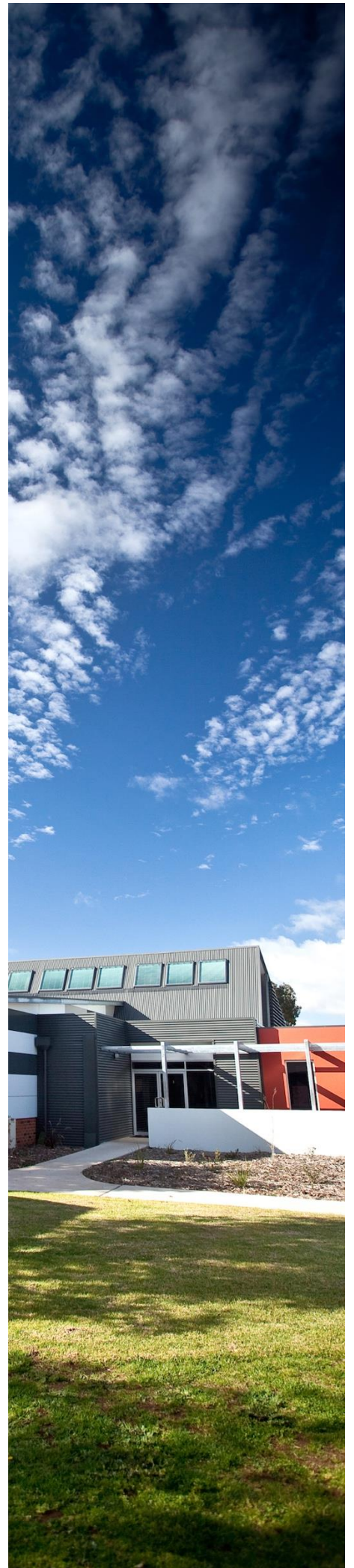




Statement of Environmental Effects

Torrens Title Subdivision
(1 Lot into 5 Lots)
1B Sydney Road
Mudgee, NSW 2850

(Our Reference: 38914-PR01_A)
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Report Title:	Statement of Environmental Effects
Project Name:	Torrens Title Subdivision (1 Lot into 5 Lots) at 1B Sydney Road, Mudgee
Client:	Canonbar Investments Pty Ltd
Project No.	38914
Report Reference	38914-PR01_A
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1 INTRODUCTION

1.1 Background

Barnson Pty Ltd has been engaged by Canonbar Investments Pty Ltd to prepare information in support of a Development Application (DA) for the Torrens Title subdivision (1 Lot into 5 Lots) of Lot 2 DP 1336929, known as 1B Sydney Road, Mudgee NSW 2850.

The subject site is located on the eastern side of Sydney Road and has an area of 3,696m². The site contains vacant grasslands with some scattered trees along the south eastern boundary.

The project will consist of the Torrens Title subdivision of Lot 2 DP 1336929 into five (5) Lots. The Lots are to be used for residential purposes.

The site is zoned R1 General Residential pursuant to the provisions under the *Mid-Western Regional Local Environmental Plan 2012* (LEP). The proposed development is defined as a 'subdivision', which is permissible with consent in the R1 zone pursuant of clause 2.6 of the LEP.

This application consists of:

- A completed development application form; and
- PDF copy of this written statement, including plans and supporting documents.

1.2 Proponent

The proponent for the DA is Canonbar Investments Pty Ltd.

1.3 Consultant

Barnson Pty Ltd

Jack Massey

Suite 8 / 11 White Street

Tamworth NSW 2340

2 EXISTING ENVIRONMENT

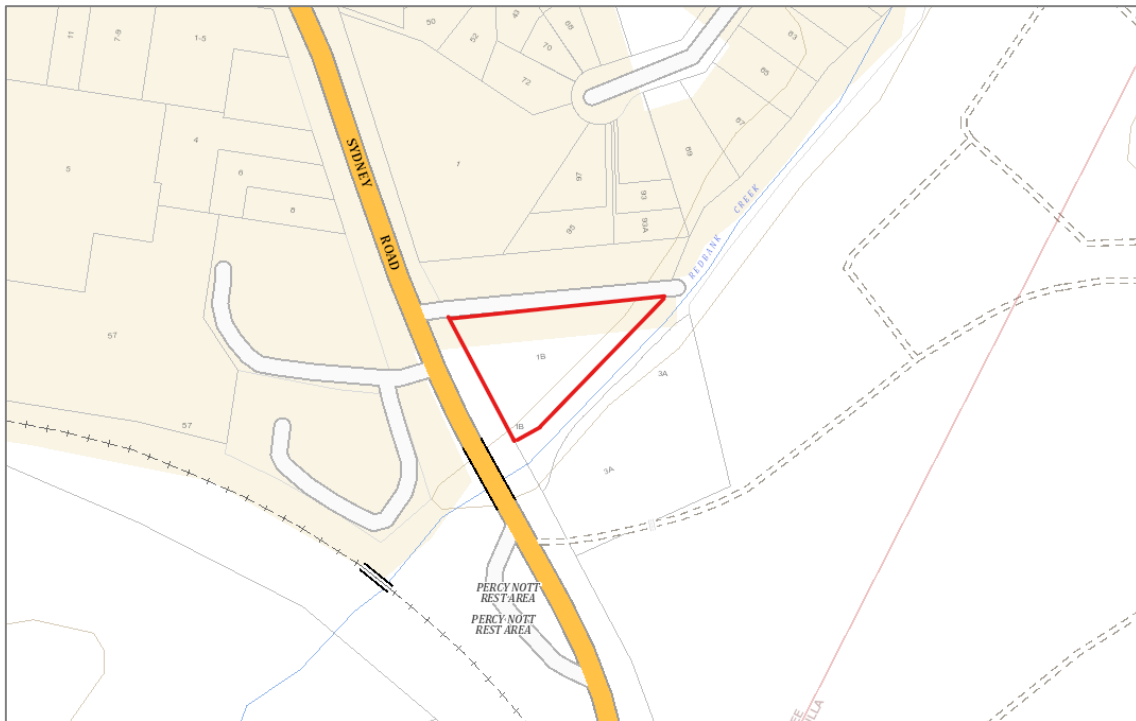
2.1 Location and Title

The subject site of this application is Lot 2 DP 1136929, known as 1B Sydney Road, Mudgee. Details of the site has been provided in **Table 1** below.

Table 1 – Site Details

Street No.	Lot	Road Frontage	Area	Use of land
1B	Lot 2 in DP 1136929	Sydney Road & Right of Carriageway	3696m ²	Vacant land

The site is located on the eastern side of Sydney Road on the outskirts of Mudgee, as shown in **Figure 1** below.



Source: (NSW Government Spatial Services, 2022)

Figure 1 – Site Location

The site has an overall area of 3,696m² (refer to Deposited Plan in **Appendix A** of this report) with direct frontage to Sydney Road.

The subject site is not developed and consists of vacant managed grasslands and scattered trees along the southeast boundary line, as shown in **Figure 2**.



Source: (NSW Government Spatial Services, 2022)

Figure 2 – Site Aerial

2.2 Land Use

The subject site is located within an urban area characterised by existing residential dwellings, units and scattered commercial/industrial land uses. The Arden Hotel and The Laneway (self-contained accommodation) are located to the north, Mudgee Carport Court to the west and vacant lands to the south and east. There are residential dwellings scattered throughout the locality. The site is vacant land with no known historic improvements or uses.

2.3 Topography

The subject site and adjoining lands are relatively flat throughout.

2.4 Flora and Fauna

The site contains managed grassland vegetation with scattered trees along the southeast boundary line. Given the urban locality of the site, there is minimal fauna present.

2.5 Natural Hazards

2.5.1 Flooding

The subject site is mapped within the Flood Planning Area pursuant to the NSW ePlanning Spatial Viewer and *Mid-Western Regional Local Environmental Plan 2012*. The flood area is identified along the southeast boundary where the subject site is adjacent to Redbank Creek (refer to **Figure 3** below).

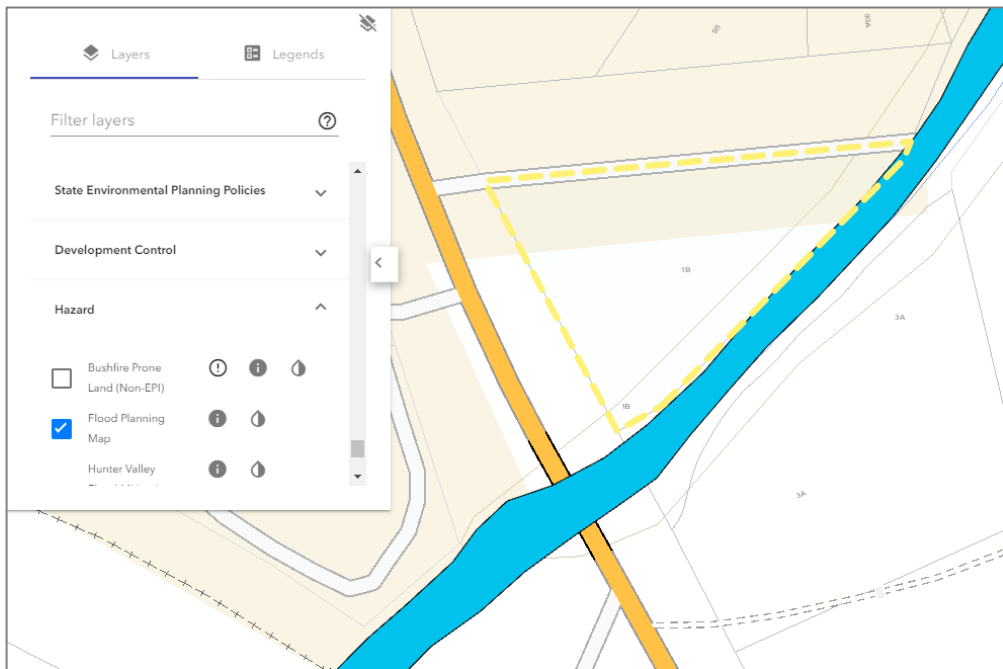


Figure 3 – Flood Planning Mapping

2.5.2 Bush Fire

The subject site is not mapped as bushfire prone land pursuant to the NSW ePlanning Spatial Viewer or the RFS bushfire mapping system.

2.6 Services

The subject site is connected to existing services including reticulated water and sewer, stormwater, electricity and telecommunications.

The subject site contains a Right of Carriageway along the northern boundary, as shown on the Deposited Plan in **Appendix A** of this report.

2.7 Access and Traffic

The subject site has frontage to Sydney Road which is a sealed road improved with kerb and gutter. A Right of Carriage (ROC) is established along the northern boundary of the site, which is

also applicable to the southern boundary of the neighbouring Lot 1 SP96405, known as 1A Sydney Road Mudgee. The ROC provides access to both properties.

Sydney Road, also known as the Castlereagh Highway, is a Transport for NSW classified road (No. 18) pursuant to the *NSW RMS Schedule of Classified Roads and Unclassified Roads*. The road has an approximate width of 13m wide and facilitates one (1) traffic flow lane in each direction. The speed limit along this portion of the road is 50km/h.

2.8 Heritage

The site is not identified in Schedule 5 of the *Mid-Western Regional Local Environmental Plan 2012* or the State Heritage Inventory as containing a heritage listed item.

An Aboriginal Heritage Information System (AHIMS) search was undertaken for the site and immediate surrounds. The AHIMS search revealed that there are no Aboriginal sites recorded within 200m of the subject site. Refer to AHIMS search in **Appendix B** of this report

3 PROPOSED DEVELOPMENT

The proposed development involves the Torrens Title subdivision of Lot 2 DP 1136929 into five (5) lots. Plans of the proposed subdivision are provided in **Appendix C** of this report.

The intent of the proposed subdivision is to provide new Lots suitable for future residential accommodation. The details of the proposed new Lot arrangement is provided in **Table 2** below.

Table 2 – Subdivision Details

Proposed Lot	Road Frontage/Access	Area	Use of land
Lot 1	Existing Right of Carriageway connecting to Sydney Road	816m ²	Residential
Lot 2	Existing Right of Carriageway connecting to Sydney Road	804m ²	Residential
Lot 3	Existing Right of Carriageway connecting to Sydney Road	800m ²	Residential
Lot 4	Existing Right of Carriageway connecting to Sydney Road Battle-axe arrangement	672m ²	Residential
Lot 5	Existing Right of Carriageway connecting to Sydney Road Battle-axe arrangement	604m ²	Residential

The proposed subdivision is for a Torrens Title subdivision. Each proposed lot shall be improved with the provision of essential services and have access and direct frontage to an access road.

The proposed subdivision shall contain the following services;

- Reticulated water provided with water main along the Right of Carriageway;
- Sewer and stormwater is to be directed to the south eastern boundary into a new junction;
- Electrical and Telecommunications infrastructure are available to the subject site;
- Access via the Right of Carriage afforded to the existing lot along the north boundary.

The proposed development includes associated earthworks to provide the essential services infrastructure.

The services has been strategically positioned to avoid any existing vegetation on the subject site. As such, there shall be no removal of trees or bushes as part of subdivision works.

The proposed development will include easements for the essential service infrastructure located on the subject site, as shown in Servicing Plans attached in **Appendix C** of this report.

4 LAND USE ZONING

The subject site is zoned R1 pursuant to *Mid-Western Local Environmental Plan 2012* (LEP). The proposed development is for a subdivision, which is permissible with consent in the R1 Zone pursuant to clause 2.6 of the LEP.

The permissibility of the proposed development is assessed in terms of the heads of consideration in Section 4.15 of the *Environmental Planning & Assessment Act 1979*, which incorporates consideration of the LEP and the objectives and permissible uses outlined in the R1 Zone, as outlined in **Section 5** of this report.

5 PLANNING CONSIDERATIONS

5.1 Water Management Act 2000

A controlled activity approval is required as the development involves works on waterfront land and the discharge of stormwater into a watercourse. Under the *Water Management Act 2000* (WM Act), a controlled activity means the following:

- (a) *the erection of a building or the carrying out of a work (within the meaning of the [Environmental Planning and Assessment Act 1979](#)), or*
- (b) *the removal of material (whether or not extractive material) or vegetation from land, whether by way of excavation or otherwise, or*
- (c) *the deposition of material (whether or not extractive material) on land, whether by way of landfill operations or otherwise, or*
- (d) *the carrying out of any other activity that affects the quantity or flow of water in a water source.*

Water front includes the bed of any river, lake or estuary and all land within 40 metres of the highest bank of the river, lake or estuary. The proposal involves minor works within 40m of waterfront land and the discharge of stormwater into the watercourse.

The development also involves the following in the vicinity of the watercourse:

- Installation of sewer mains and stormwater lines.

As a result, the application is lodged as integrated development seeking General Terms of Approval from NSW Office of Water.

5.2 Biodiversity Conservation Act 2016

5.2.1 Is the development likely to significantly affect threatened species?

Clause 7.2 of the *Biodiversity Conservation Act 2016* (BC Act) identifies the following circumstances where a development is likely to significantly affect threatened species:

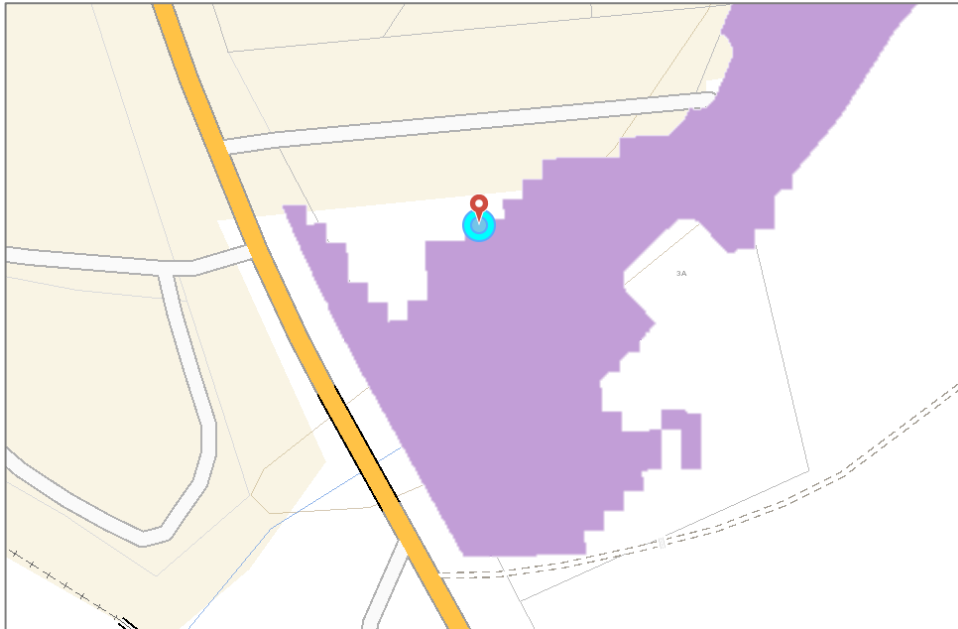
- (a) *it is likely to significantly affect threatened species or ecological communities, or their habitats, according to the test in section 7.3, or*
- (b) *the development exceeds the biodiversity offsets scheme threshold if the biodiversity offsets scheme applies to the impacts of the development on biodiversity values, or*
- (c) *it is carried out in a declared area of outstanding biodiversity value.*

The Biodiversity Values Mapping indicates that a small portion of the site along the southern boundary may contain areas of Biodiversity Value, (refer to **Figure 5** below), however given the development is for a subdivision, no clearing of vegetation will be required in this area to accommodate the proposed development. The proposed servicing shall be setback from the existing vegetation on the site so it does not require any clearing

to allow the services to be installed. Therefore, the proposed development does not exceed the biodiversity offsets threshold for the purposes of this Section of the Act.

5.2.1.1 Declared Area of Outstanding Biodiversity Value

The site is mapped on the Biodiversity Value Map as being land with a high biodiversity value as defined by the BC Act.



Source: (NSW Government, 2022)

Figure 4 – Biodiversity Value Map

The site is not identified as being within an area of Outstanding Biodiversity Value.

5.2.2 Biodiversity Development Assessment Report

As outlined in **Section 5.2.1**, the proposed development is not likely to significantly affect threatened species as defined by Section 7.2 of the BC Act. Therefore, a Biodiversity Development Assessment Report is not required to accompany the application for development consent.

5.3 Fisheries Management Act 1994

5.3.1 Applicability

The Fisheries Management Act 1994 (FM Act) applies to:

- (a) *in relation to all waters that are within the limits of the State, and*
- (b) *except for purposes relating to a fishery, or a part of a fishery, that is to be managed in accordance with the law of the Commonwealth pursuant to an arrangement under Division 3 of Part 5 and except for purposes prescribed by paragraph (d)—in relation to*

any waters of the sea not within the limits of the State that are on the landward side of waters adjacent to the State that are within the Australian fishing zone, and

- (c) for purposes relating to a fishery, or a part of a fishery, that is managed in accordance with the law of the State pursuant to an arrangement under Division 3 of Part 5—in relation to any waters to which the legislative powers of the State extend with respect to that fishery, whether pursuant to section 5 of the Coastal Waters (State Powers) Act 1980 of the Commonwealth or otherwise, and*
- (d) for purposes relating to recreational fishing activities engaged in otherwise than by use of a foreign boat (other than recreational activities prohibited or regulated under a plan of management determined under section 17 of the Commonwealth Act)—in relation to any waters to which the legislative powers of the State extend with respect to such activities.*

Comment: Not applicable to the proposed development

5.4 Environmental Planning & Assessment Act 1979

5.4.1 Application of Biodiversity Conservation Act 2016 & Fisheries Management Act 1994

Section 1.7 of the *Environmental Planning & Assessment Act 1979* (EP&A Act) identifies that Part 7 of the BC Act and Part 7A of the FM Act relate to the operation of the EP&A Act in relation to the terrestrial and aquatic environment. These Acts are addressed in **Sections 5.1** and **5.3** of this report respectively.

5.4.2 Evaluation

Section 4.15 of the EP&A Act (as amended) requires the Council to consider various matters in regard to the determination of the Development Application.

In determining a development application, a consent authority is to take into consideration such of the following matters as are of relevance to the development the subject of the development application:

- (a) The provisions of:*
 - (i) any environmental planning instrument, and*
 - (ii) any proposed instrument that is or has been the subject of public consultation under this Act and that has been notified to the consent authority (unless the Secretary has notified the consent authority that the making of the proposed instrument has been deferred indefinitely or has not been approved), and*
 - (iii) any development control plan, and*
 - (iiia) any planning agreement that has been entered into under section 7.4, or any draft planning agreement that a developer has offered to enter into under section 7.4, and*
 - (v) the regulations (to the extent that they prescribe matters for the purposes of this paragraph), and*

- (v) *any coastal zone management plan (within the meaning of the Coastal Protection Act 1979), that apply to the land to which the development application relates,*
- (b) *The likely impacts of that development, including environmental impacts on both the natural and built environments, and social and economic impacts in the locality;*
- (c) *The suitability of the site for the development,*
- (d) *Any submissions made in accordance with this act or the regulations,*
- (e) *The public interest.*

The proposed development has been designed with consideration to the following matters, as outlined below.

5.4.3 Integrated Development

Development that requires both development consent and another approval listed under Section 4.46 of the EP&A Act is 'Integrated Development'. The proposed development is Integrated Development by virtue of requiring:

- An Activity Approval under the *Water Management Act 2000*. Pursuant to Section 91 of the Act the development is required to obtain a controlled activity approval;
- Connect a road (whether public or private) to a classified road under the *Roads Act 1993*. Refer to Traffic Impact and Parking Assessment in **Appendix D** of this report

5.5 Environmental Planning Instruments

5.5.1 SEPP (Biodiversity and Conservation) 2021

Whilst the subject site is located within the Mid-Western Regional LGA, it is not considered to comprise potential koala habitat as defined by *State Environmental Planning Policy (Koala Habitat Protection) 2020*. The proposed development does not include significant clearing of vegetation or erection/construction of any buildings or structures. The proposed development does not significantly impact negatively upon any local foreshore or watercourse. Therefore SEPP (Biodiversity and Conservation) 2021 does not require any further consideration.

5.5.2 SEPP (Resilience and Hazards) 2021

Clause 4.6 of *State Environmental Planning Policy (Resilience and Hazards)* requires Council to consider the following before granting consent to a DA:

- (a) *it has considered whether the land is contaminated, and*
- (b) *if the land is contaminated, it is satisfied that the land is suitable in its contaminated state (or will be suitable, after remediation) for the purpose for which the development is proposed to be carried out, and*

- (c) *if the land requires remediation to be made suitable for the purpose for which the development is proposed to be carried out, it is satisfied that the land will be remediated before the land is used for that purpose.*

Comment: There are no known historical contaminating activities or land uses that have been historically carried out on the subject site or within proximity. Therefore, no further consideration of this SEPP is required and it is assumed that the site is suitable for the proposed subdivision and residential future land use.

5.5.3 SEPP (Transport and Infrastructure) 2021

Clause 2.118 'Development with frontage to classified road' applies to the proposed development as the existing lot on 1B Sydney Road, Mudgee has direct frontage to Sydney Road (A Classified Road). Clause 2.118 requires council to be satisfied with the following before granting development consent:

- (a) *where practicable and safe, vehicular access to the land is provided by a road other than the classified road, and*
- (b) *the safety, efficiency and ongoing operation of the classified road will not be adversely affected by the development as a result of—*
- (i) *the design of the vehicular access to the land, or*
 - (ii) *the emission of smoke or dust from the development, or*
 - (iii) *the nature, volume or frequency of vehicles using the classified road to gain access to the land, and*
- (c) *the development is of a type that is not sensitive to traffic noise or vehicle emissions, or is appropriately located and designed, or includes measures, to ameliorate potential traffic noise or vehicle emissions within the site of the development arising from the adjacent classified road.*

Comment: The proposed development has safe access provided via the existing Right of Carriageway (ROC) which connects to Sydney Road. A Traffic Impact Assessment has been prepared and is provided in **Appendix D** of this report.

5.5.4 Mid-Western Regional Local Environmental Plan 2012

5.5.4.1 Subdivision - Consent Requirements

Clause 2.6 of the *Mid-Western Regional Local Environmental Plan 2012* (the LEP) permits the subdivision of land with development consent. Thus, the proposed development is permissible with consent subject to other relevant provisions of the LEP as outlined below.

5.5.4.2 Land Use Table

The subject site is zoned R1 General Residential pursuant to *Mid-Western Regional Local Environmental Plan 2012* (LEP). The objectives of the R1 zone are:

- *To provide for the housing needs of the community.*
- *To provide for a variety of housing types and densities.*
- *To enable other land uses that provide facilities or services to meet the day to day needs of residents.*

Comment: The proposed development is defined as a 'subdivision', which is considered to be consistent with the zone objectives as it provides opportunity for further development, contributing to providing for the housing needs of the community and variety of housing types and densities. It is permissible with consent in the R1 zone.

5.5.4.1 Minimum Lot Size

Clause 4.1 of the LEP states:

(3) The size of any lot resulting from a subdivision of land to which this clause applies is not to be less than the minimum size shown on the [Lot Size Map](#) in relation to that land.

The minimum lot size (MLS) for the subject site is 600sqmas per the Lot Size Map

Comment: The subject site enjoys a MLS of 600m². The five (5) proposed lots all meet the minimum requirement of 600m², as shown on the subdivision plans in **Appendix C** of this report.

5.5.4.2 Flood Planning

The subject site is mapped as being within the Flood Planning Area. Clause 7.1 of the LEP states that consent cannot be granted for development in a flood planning area unless the consent authority is satisfied that:

- is compatible with the flood hazard of the land, and*
- will not significantly adversely affect flood behaviour resulting in detrimental increases in the potential flood affectation of other development or properties, and*
- incorporates appropriate measures to manage risk to life from flood, and*
- will not significantly adversely affect the environment or cause avoidable erosion, siltation, destruction of riparian vegetation or a reduction in the stability of river banks or watercourses, and*
- is not likely to result in unsustainable social and economic costs to the community as a consequence of flooding.*

Comment: The site is mapped as containing a minor section of land within the Flood Planning Area under the LEP (refer to **Figure 5** below). The proposed development, being a subdivision, will not extend into the portion of the site identified as being within the Flood Planning Area.

However, whilst not within the Flood Planning Area or below the Flood Planning Level, the “Mudgee Local Creeks Floodplain Risk Management Study and Plan” indicates a portion of the site is within the Probable Maximum Flood area. Future dwellings on the subject site shall consider the requirements under this study and plan in order to ensure that the finished floor level of those dwellings are higher than the top of the bank of the adjacent creek, which would be the extent of the 1 in 100 years flood event.

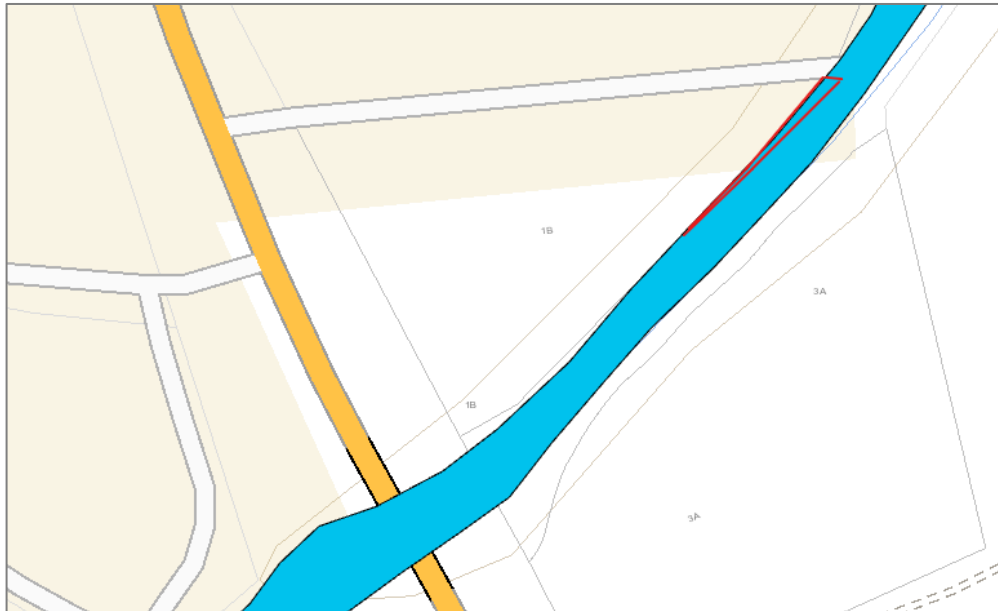


Figure 5 – Flood Planning Area Mapping

5.5.4.3 Groundwater Vulnerability

Clause 6.4 ‘Groundwater Vulnerability’ applies to the subject application as the subject site is mapped with groundwater vulnerabilities according to NSW Governments ePlanning Spatial Viewer.

Clause 6.4 of the LEP states:

(3) Before determining a development application for development on land to which this clause applies, the consent authority must consider the following—

(a) the likelihood of groundwater contamination from the development (including from any on-site storage or disposal of solid or liquid waste and chemicals),

(b) any adverse impacts the development may have on groundwater dependent ecosystems,

(c) the cumulative impact the development may have on groundwater (including impacts on nearby groundwater extraction for a potable water supply or stock water supply),

(d) any appropriate measures proposed to avoid, minimise or mitigate the impacts of the development.

Comment: There are no proposed groundwater contamination activities, or any development that would have adverse impacts on groundwater dependant ecosystems. There are no identified cumulative impacts on groundwater or nearby groundwater extraction of the proposed development and as such, is considered to be compliant for the purposes of this part.

5.5.4.4 Essential Services

Clause 6.9 ‘Essential Services’ applies to the subject site, as each lot is required to have essential services available or adequate arrangements in place to make them available when required.

Clause 6.9 of the LEP states:

Development consent must not be granted to development unless the consent authority is satisfied that any of the following services that are essential for the proposed development are available or that adequate arrangements have been made to make them available when required—

- (a) the supply of water,
- (b) the supply of electricity,
- (c) the disposal and management of sewage,
- (d) stormwater drainage or on-site conservation,
- (e) suitable road access.

Comment: All essential services including reticulated water, sewer and stormwater as well as electrical and telecommunications infrastructure is available to the subject site, and includes easements for associated essential service infrastructure located on the subject site. Refer to Subdivision Plans in **Appendix C** of this report.

All proposed lots have suitable road access to Sydney Road via the ‘Right of Carriage’ way afforded along the north boundary of the existing lot (all proposed lots have direct access and frontage to the right of carriage) providing suitable road access to all proposed lots.

5.6 Draft Environmental Planning Instruments

No draft Environmental Planning Instruments are applicable to the subject site or development.

5.7 Development Control Plans

The *Mid-Western Regional Environmental Plan 2013* applies to the subject site. The relevant provisions have been addressed in **Table 3** below.

Table 3 – DCP Compliance Table	
Provision	Comment
<i>Section 5.3 Stormwater Management</i>	

Section 5.3 of the DCP outlines the requirements for stormwater management. A new stormwater line will be installed along the rear boundary, which will be dispersed in accordance with Council's requirements.

Any future development on the lots would be required to comply with the requirements of the DCP for construction and operation. All hardstand will be directed with adequate falls towards the kerb and gutter to the street networks, included the ROC.

Approvals in accordance with the *Water Management Act 2000* shall be obtained as part of the Integrated Development process.

Section 5.54 Environmental Controls

Protection of Aboriginal Archaeology Items	An AHIMS search has been undertaken for the site and its immediate surrounds (refer to Appendix B of this report). There are no items of Aboriginal Heritage significance located on the site or within proximity.
Bushfire Management	This site is not mapped as being bushfire prone.
Riparian and drainage line Environments	It is noted that there is a watercourse located to the south east of the site. Riparian vegetation generally consists of small shrubs and established trees along the watercourse. The riparian vegetation appears to be in good health and shall be protected where appropriate. Stormwater quality shall be monitored to ensure that this vegetation and any future vegetation is not affected by the proposed subdivision. Erosion and sediment control measures shall be implemented as part of future applications for residential occupation. Once stormwater drainage works have been completed, revegetation of the corridor shall be established, if required and in accordance with Office of Water General Terms of Approval.
Pollution and Waste Management	The proposed development would not result in pollution of the environment. The residential lots would be connected to reticulated sewerage and stormwater management infrastructure.
Threatened Species and Vegetation Management	The site has been heavily disturbed as a result of previous activities on the site. There would be no trees removed as part of the proposed development.
Building in Saline Environments	The site is not known to be saline.

Section 7.1 Urban Subdivision: Land zoned R1, R2 and R3

Lot Size	The minimum lot size (as determined by the Mid-Western Regional LEP 2012) is 600m ² . The proposed Lots meets the specified MLS. The subject site is not impacted by slope controls as topography of subject site is relatively flat with a slight fall that is under 10 degrees.
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	<p>16m widths at building lines (4.5m from front of property boundaries) can be achieved on all proposed lots.</p> <p>Departure Request</p> <p>The DCP requires all lots to have street frontage. The proposed development produces five new allotments with two lots having street frontage to Sydney Road as well as having frontage to the right of carriage afforded to the existing site along the northern boundary. The remaining three (3) new allotments only have frontage to the right of carriage along the northern boundary (which gives access to Sydney Road).</p> <p>The proposed development requests a variation from this provision on the following grounds;</p> <p>Justification for Variation</p> <p>The Right of Carriageway (ROC) is the most appropriate solution to provide access to the proposed development to have the minimal impact on the local road network. The TIA (attached in Appendix D) shows that the ROC meets the objectives of the relevant provisions regarding traffic and parking, minimizes the impacts on the local road network and provides safe and practical access to all the proposed lots.</p> <p>The proposed lots will have frontage to an access road, although it is not considered a ‘street frontage’, the objectives of this provision are considered to be met with the proposed development affording frontage to the ROC.</p>
Lot Design	<p>The proposed development takes into consideration the lot orientation in regard to optimizing solar access while also considering the existing solar patterns and orientation.</p> <p>Proposed lots 2,3,4 and 5 are all east/west orientated with an increased width and midpoint with sunlight access to a minimum of three hours between 9:00am and 3:00pm on the 21st June (Winter Solstice).</p> <p>Proposed lot 1 is north/south orientated within the range of 30 degrees east of north and 20 degrees west of north.</p> <p>All proposed lots are generally rectangular in shape and all corner lots (Lot 1) has sufficient area to allow development for the purposes of a dual occupancy.</p>
Street Design and Layout	<p>A Traffic Impact Assessment (TIA) is required as the proposed development involves five (5) allotments produced from the subdivision. The TIA is attached in Appendix D of this report and details the road hierarchy, how the development integrates with the locality and recommended alterations to the existing road network in the locality.</p>
Cycleways and Footpaths	Not applicable to proposed development.
Open Space	Not applicable to proposed development.

Landscaping	The subdivision does not include landscaping, only cut and fill earthworks.
Utility Services	A Servicing Plan is attached in Appendix C which includes the provisions of reticulated sewer and water services, drainage and stormwater management systems. Electricity and telecommunications service connections are identified in the Existing site Plan provided in Appendix C .
Drainage	It is considered that the proposed service plan enables the capacity for future development to meet the required stormwater and drainage provisions, and future development that includes the construction and maintenance of service infrastructure will have the capacity to adhere to the controls and provisions listed in the DCP.

5.8 Any Planning Agreement entered into

No Planning Agreements entered into are known to exist in relation to the development or site.

5.9 Any Matters Prescribed by the Regulations

For the purposes of Section 4.15(1)(a)(iv) of the EP&A Act, Clause 61 of the *Environmental Planning and Assessment Regulations 2021* (EP&A Regulations) specifies the additional matters a consent authority must take into consideration when determining a DA. There are no provisions relevant to the proposed development.

5.10 Any Likely Impacts of the Development

5.10.1 Context & Setting

The subject site is located on the outskirts of Mudgee in an area that intersects from urban to rural. It is characterised by residential and commercial land uses within proximity. The proposed development would be consistent with the existing context and setting as it will reflect the R1 General Residential land use zoning of the site. Furthermore, the intent of the subdivision is to provide affordable housing for the community and Mudgee housing market. The provision of affordable housing shall diversify the Mudgee housing market by providing affordable and sizeable blocks intended for residential accommodation in a convenient and accessible location.

5.10.2 Access, Transport & Traffic

The proposed lots all have direct frontage to the right of carriage way that connects to Sydney Road, with Lots 1, 2 and 3 having adjoining boundary lines to the right of carriage way and proposed lots 4 and 5 having frontage via access handles.

The access road/right of carriageway connecting to Sydney Road is a two way, two laned sealed bitumen road which connects to Sydney Road, a two way two laned sealed classified road.

A Traffic Impact Assessment is provided and attached in **Appendix D** of this report. A summary of the key components are:

- The ROC access road frontage is the most appropriate and least impacting solution to provide access/direct frontage to the proposed allotments.
- The proposed development will produce no detrimental impact to the performance of the intersection as a result of generated traffic

5.10.3 Utilities

The provision of all essential services including reticulated water, sewer, stormwater, electricity and telecommunications infrastructure will be available when required (refer to plans attached in **Appendix C** of this report).

5.10.4 Heritage

The subject site is not identified in Schedule 5 of the *Mid-Western Regional Local Environmental Plan 2012*. An Aboriginal Heritage Information System (AHIMS) search was undertaken for the site and immediate surrounds. The AHIMS search revealed that there are no Aboriginal sites recorded within 200m of the subject site.

5.10.5 Social and Economic Impacts in the Locality

The subdivision creates the capacity for future development that would provide for the housing needs of the growing community of Mudgee within a residential environment (consistent with its surrounding locality) and make better use of the existing vacant land mainly consisting of vacant grasslands. The proposed development promotes affordability in the middle of a competitive residential market locally with the provision of additional residential land. It also promotes diversity in residential land stock, as well as creates an increase of economic activity in the area with potential new land to develop providing employment and economic activity for local businesses in the building industry.

5.10.6 Site Design & Internal Design

There are no prohibitive constraints posed by adjacent developments. There does not appear to be any zoning, planning or environmental matters that should hinder the proposed development of the site. In this regard, it can be concluded that the proposed subdivision is suitable for the locality.

5.10.7 Other Impacts

There are no other adverse impacts that would result from the proposed subdivision.

5.11 Suitability of the Site for the Proposed Development

The suitability of the site for the proposed development has been addressed in the above sections of this report. There are no prohibitive constraints posed by adjacent developments. There does not appear to be any zoning, planning or environmental matters that should hinder the proposed development of the site. In this regard, it can be concluded that the proposal fits into the locality and the site attributes are conducive for the development.

5.12 The Public Interest

The proposed development is considered to be in the public interest as it provides for a subdivision of a vacant lot. As outlined throughout this report the development is consistent with the minimum lot size for the area and is not expected to have any adverse off-site impacts.

6 CONCLUSION

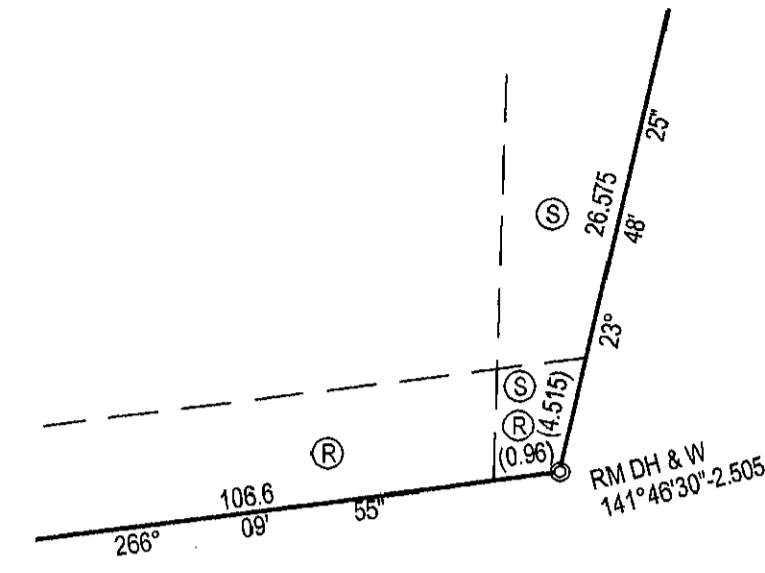
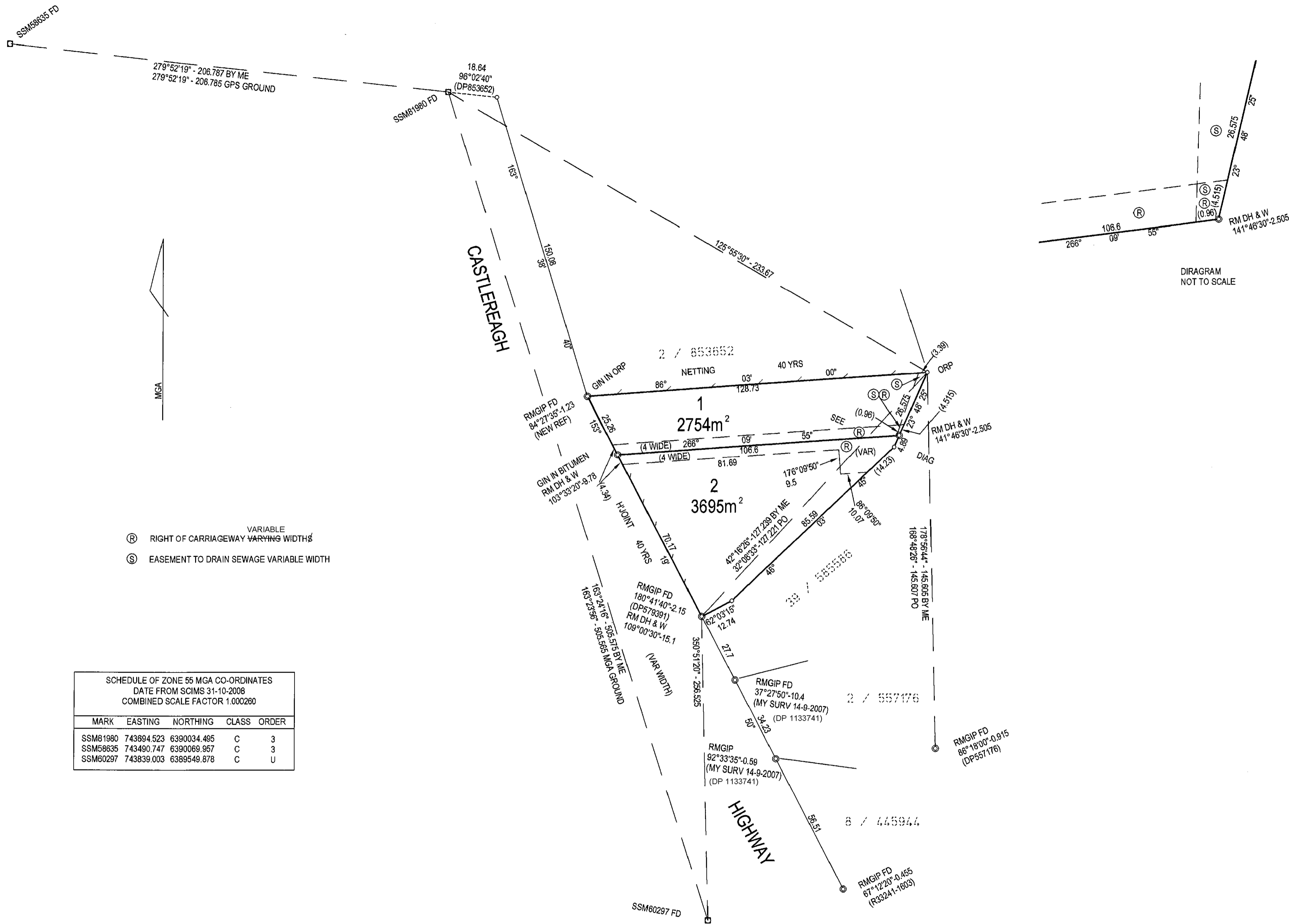
It is recommended that the proposed subdivision on Lot 2 DP 1136929 known as 1B Sydney Road, Mudgee be supported on the following grounds:

- The proposal is considered acceptable in terms of the provisions of Section 4.15 of the *Environmental Planning and Assessment Act 1979*;
- The proposal is permissible with consent and consistent with the relevant development standards and provisions of the *Mid-Western Regional Local Environmental Plan 2012*;
- The proposal complies with the relevant provisions of the *Mid-Western Regional Council Development Control Plan 2013*;
- The proposed development is not anticipated to generate any adverse impacts in the locality; and
- The proposed development is considered suitable for the site and its surrounds.

7 REFERENCES

- NSW Government. (2022, June 2). *Biodiversity Value Map*. Retrieved from <https://www.lmbc.nsw.gov.au/Maps/index.html?viewer=BVMap>
- NSW Government Spatial Services. (2022, June 2). *Six Maps*. Retrieved from <http://maps.six.nsw.gov.au/>
- NSW Rural Fire Service. (2019). *Planning for Bush Fire Protection: A Guide for Council's, Planners, Fire Authorities and Developers*. Sydney: NSW RFS.

Appendix A - Deposited Plan



DIRAGRAM NOT TO SCALE

- Ⓡ VARIABLE RIGHT OF CARRIAGEWAY VARYING WIDTHS
- Ⓢ EASEMENT TO DRAIN SEWAGE VARIABLE WIDTH


SCHEDULE OF ZONE 55 MGA CO-ORDINATES				
DATE FROM SCIMS 31-10-2008				
COMBINED SCALE FACTOR 1.000280				
MARK	EASTING	NORTHING	CLASS	ORDER
SSM81980	743894.523	6390034.495	C	3
SSM58635	743490.747	6390069.957	C	3
SSM60297	743839.003	6389549.878	C	U


Box:e-IDS /Doc:DP 1136929 P /Rev:02-Apr-2009 /Prt:03-Apr-2009 03:12 /Pgs:ALL /Seq:1 of 2
WARNING : Electronic Document Supplied by LPI NSW for Your Internal Use Only.

Surveyor : ROBERT JOHN CROOKS
 Date of Survey : 16TH NOVEMBER, 2008
 Surveyor's Ref : F182MU

PLAN OF SUBDIVISION OF LOT 35 DP579391

LGA : MID WESTERN REGIONAL COUNCIL
 Locality : MUDGEE
 Subdivision No:
 Lengths are in metres. Reduction Ratio 1: 1000

Registered
 31.3.2009


 DP1136929 P

DEPOSITED PLAN ADMINISTRATION SHEET

Sheet 1 of 1 sheet(s)

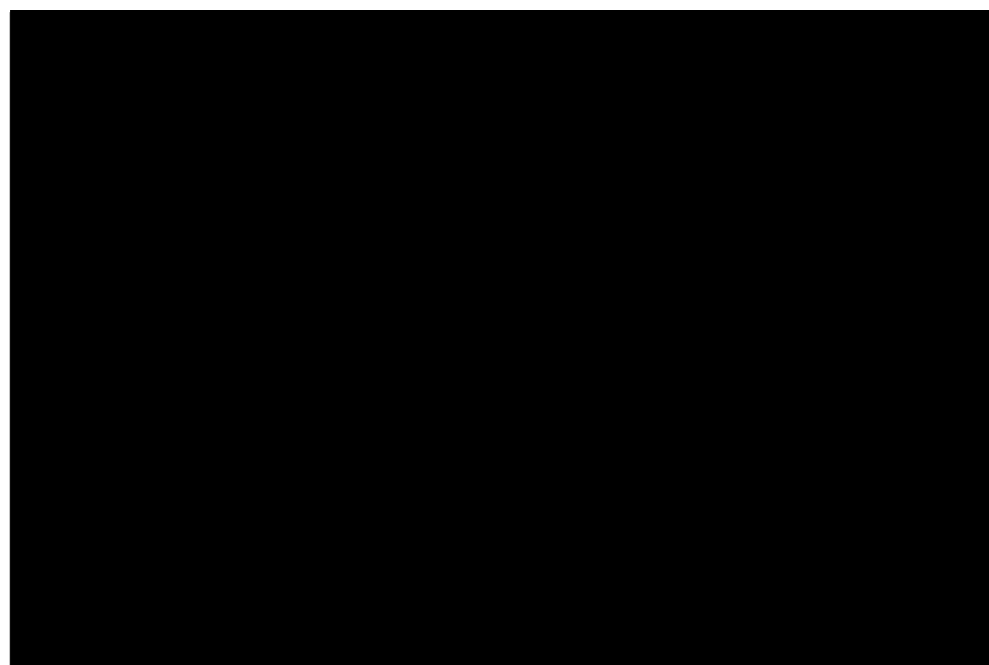
SIGNATURES, SEALS and STATEMENTS of intention to dedicate public roads, to create public reserves, drainage reserves, easements, restrictions on the use of land or positive covenants.



DP1136929 S

PURSUANT TO SECTION 88B OF THE CONVEYANCING ACT 1919 AS AMENDED, IT IS INTENDED TO CREATE:

1. RIGHT OF CARRIAGEWAY VARIABLE WIDTHS
2. EASEMENT TO DRAIN SEWAGE VARIABLE WIDTH



Registered:  31.3.2009 *

Title System: TORRENS

Purpose: SUBDIVISION

PLAN OF SUBDIVISION OF LOT 35 DP579391

LGA: MID WESTERN REGIONAL COUNCIL

Locality: MUDGEES

Parish: MUDGEES

County: WELLINGTON

Surveying Regulation, 2006

I, ROBERT JOHN CROOKS of NO. 26 MARKET STREET, MUDGEES NSW 2850 a surveyor registered under the *Surveying Act, 2002*, certify that the survey represented in this plan is accurate, has been made in accordance with the *Surveying Regulation, 2006* and was completed on: 16TH NOVEMBER, 2008

The survey ALL (specify the land actually surveyed or specify any land shown in the plan that is not the subject of the survey)

Signature  Dated: 16-11-2008
Surveyor registered under the *Surveying Act, 2002*

Datum Line: SSM81980 - SSM58635
Type: Suburban

Plans used in the preparation of survey
DP853652; DP579391; DP585586; DP557176;
DP201110; R33241-1603; MY SURVEY DATED
14-9-2007

(if insufficient space use Plan Form 6A annexure sheet)

SURVEYOR'S REFERENCE: F182MU

Use PLAN FORM 6A

for additional certificates, signatures, seals and statements

Crown Lands NSW/Western Lands Office Approval

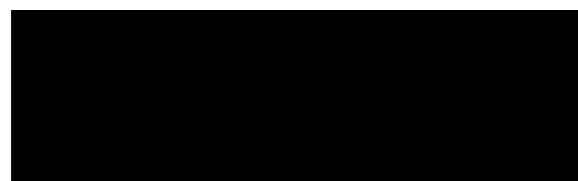
I.....in approving this plan certify
(Authorised Officer)
that all necessary approvals in regard to the allocation of the land shown herein have been given

Signature:.....
Date:.....
File Number:.....
Office:.....

Subdivision Certificate

I certify that the provisions of s.109J of the Environmental Planning and Assessment Act 1979 have been satisfied in relation to:

the proposed Subdivision set out herein
(insert 'subdivision' or 'new road')



* Authorised Person/~~General Manager/Accredited Certifier~~

Consent Authority: Mid-Western Regional Council

Date of Endorsement: 13/2/09

Accreditation no:

Subdivision Certificate no: SC046/2009

File no: PD236261 DA0044/2008

* Delete whichever is inapplicable.

* OFFICE USE ONLY

Appendix B - AHIMS

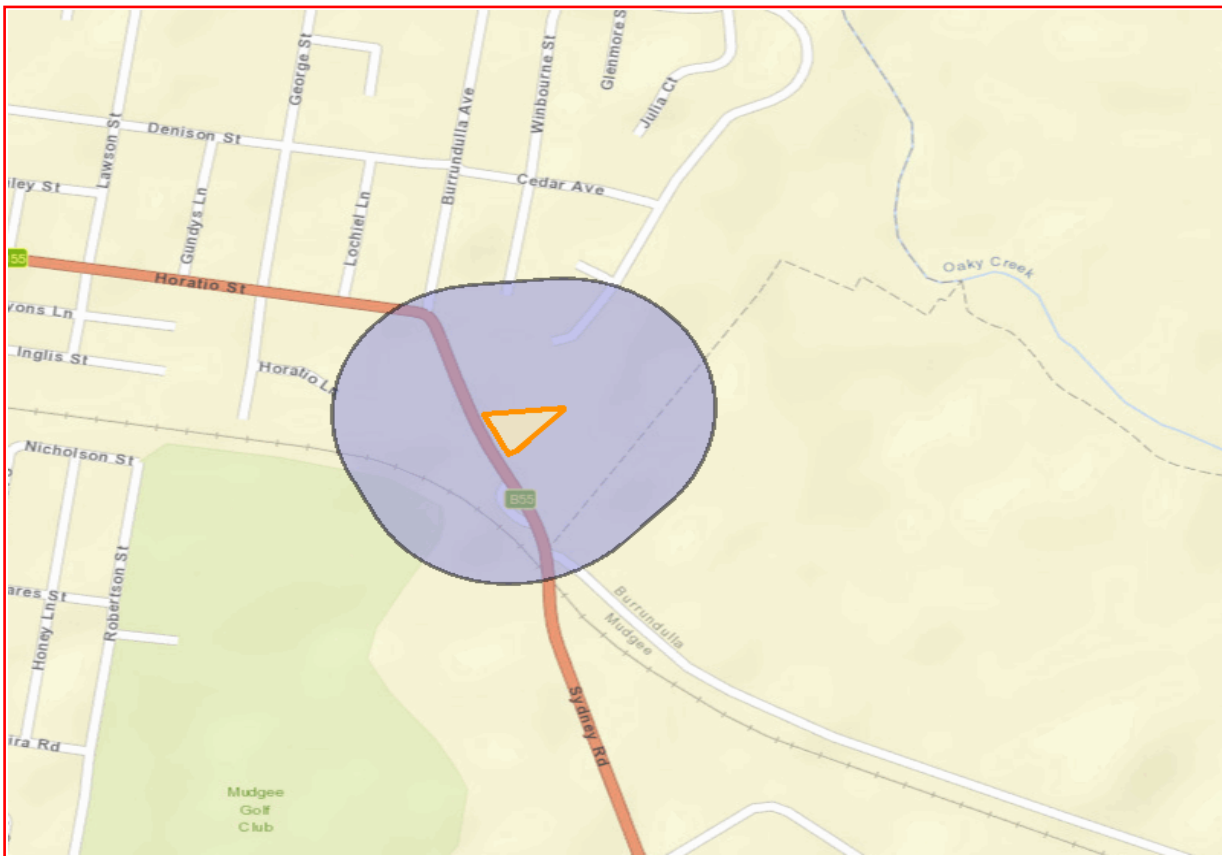
Luke Bonnell
539 Armidale Road
East Tamworth New South Wales 2340
Attention: Luke Bonnell
Email: lbonnell@barnson.com.au

Date: 02 June 2022

Dear Sir or Madam:

AHIMS Web Service search for the following area at Lot : 2, DP:DP1136929, Section : - with a Buffer of 200 meters, conducted by Luke Bonnell on 02 June 2022.

The context area of your search is shown in the map below. Please note that the map does not accurately display the exact boundaries of the search as defined in the paragraph above. The map is to be used for general reference purposes only.



A search of Heritage NSW AHIMS Web Services (Aboriginal Heritage Information Management System) has shown that:

0	Aboriginal sites are recorded in or near the above location.
0	Aboriginal places have been declared in or near the above location. *

If your search shows Aboriginal sites or places what should you do?

- You must do an extensive search if AHIMS has shown that there are Aboriginal sites or places recorded in the search area.
- If you are checking AHIMS as a part of your due diligence, refer to the next steps of the Due Diligence Code of practice.
- You can get further information about Aboriginal places by looking at the gazettal notice that declared it. Aboriginal places gazetted after 2001 are available on the [NSW Government Gazette \(https://www.legislation.nsw.gov.au/gazette\)](https://www.legislation.nsw.gov.au/gazette) website. Gazettal notices published prior to 2001 can be obtained from Heritage NSW upon request

Important information about your AHIMS search

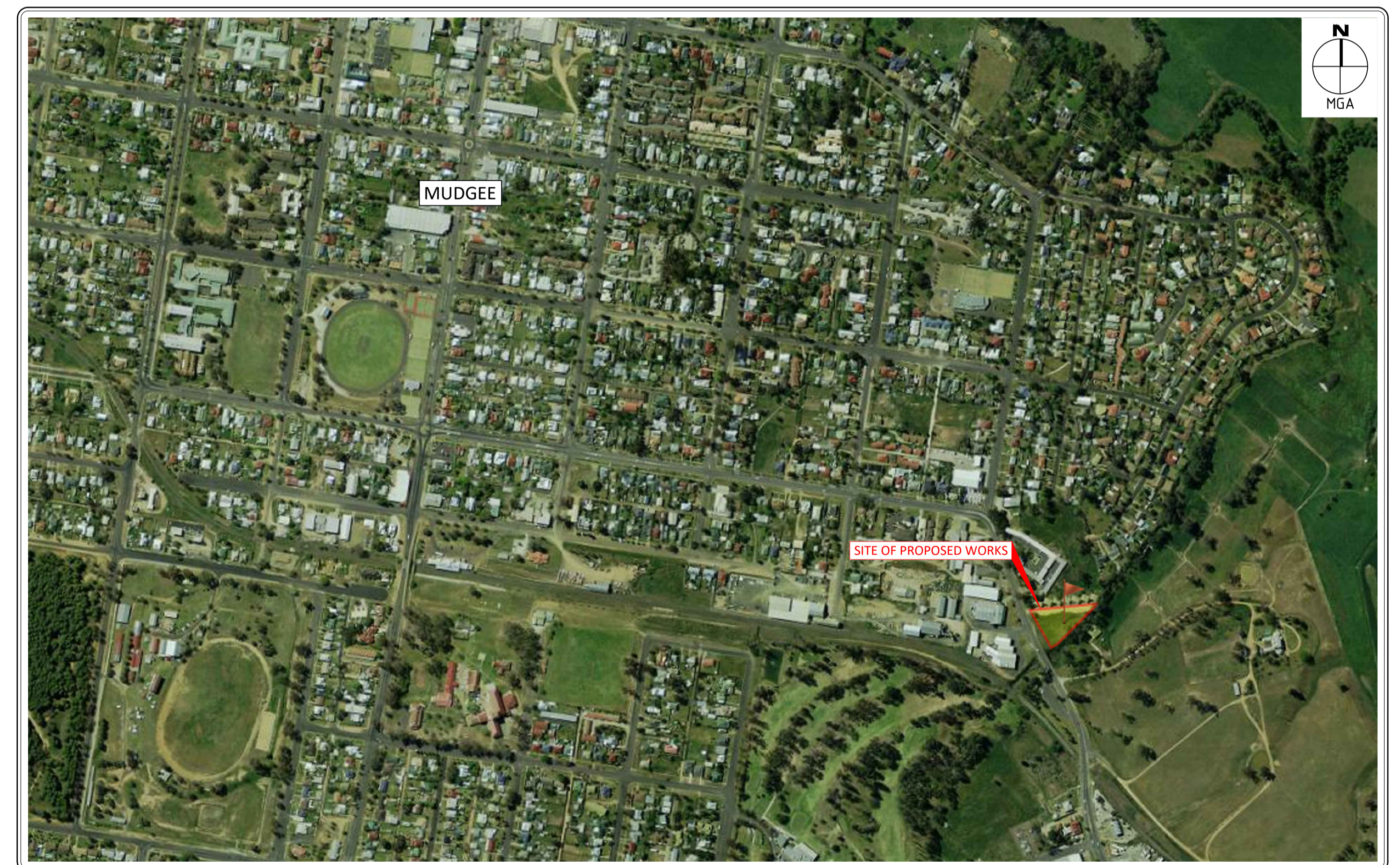
- The information derived from the AHIMS search is only to be used for the purpose for which it was requested. It is not to be made available to the public.
- AHIMS records information about Aboriginal sites that have been provided to Heritage NSW and Aboriginal places that have been declared by the Minister;
- Information recorded on AHIMS may vary in its accuracy and may not be up to date. Location details are recorded as grid references and it is important to note that there may be errors or omissions in these recordings,
- Some parts of New South Wales have not been investigated in detail and there may be fewer records of Aboriginal sites in those areas. These areas may contain Aboriginal sites which are not recorded on AHIMS.
- Aboriginal objects are protected under the National Parks and Wildlife Act 1974 even if they are not recorded as a site on AHIMS.
- This search can form part of your due diligence and remains valid for 12 months.

Appendix C - Development Plans

Development Application Documentation for the Proposed Subdivision of 1B Sydney Road, Mudgee, NSW 2850

SCHEDULE OF DRAWINGS

SHEET No.	DESCRIPTION
34914-C00	CIVIL ENGINEERING COVER SHEET
34914-C01	EXISTING SITE PLAN
34914-C02	PROPOSED SITE PLAN
34914-C03	PROPOSED LOT PLAN
34914-C04	PROPOSED STORMWATER PLAN
34914-C05	PROPOSED SEWER PLAN
34914-C06	PROPOSED WATER PLAN



LOCALITY PLAN
NOT TO REDUCTION RATIO

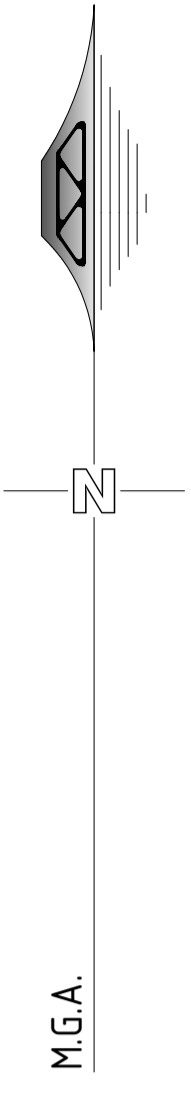
ISSUED FOR APPROVAL



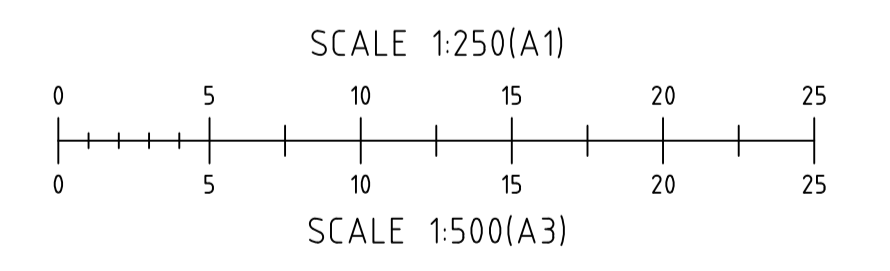
LEGEND

- EXISTING SUBJECT CADASTRAL BOUNDARIES
- - - EXISTING EASEMENT
- / — EXISTING FENCE LINE
- OE — OE — EXISTING OVERHEAD ELECTRICITY LINE
- PP EXISTING POWER POLE
- ✱ LP EXISTING LIGHT POLE
- S — S — EXISTING UNDERGROUND SEWER PIPE
- ⊙ SMH EXISTING SEWER MANHOLE

(R) RIGHT OF CARRIAGEWAY VARIABLE WIDTH (DP 1136929)
 (S) EASEMENT TO DRAIN SEWAGE VARIABLE WIDTH (DP 1136929)



EXISTING SITE PLAN
 REDUCTION RATIO 1:250 @ A1
 1:500 @ A3



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Client: **ROBERT KENNEDY**
 Project: **PROPOSED SUBDIVISION OF 1B SYDNEY ROAD MUDGEE 2850**
 Drawing Title: **EXISTING SITE PLAN**

Rev	Date	Amendment
A	16-06-2022	ISSUED FOR APPROVAL

Design	LM	Certification	
Drawn	MK		
Check	LM	Drawing Number	
Original Sheet Size = A1		38914 - C01	Revision A

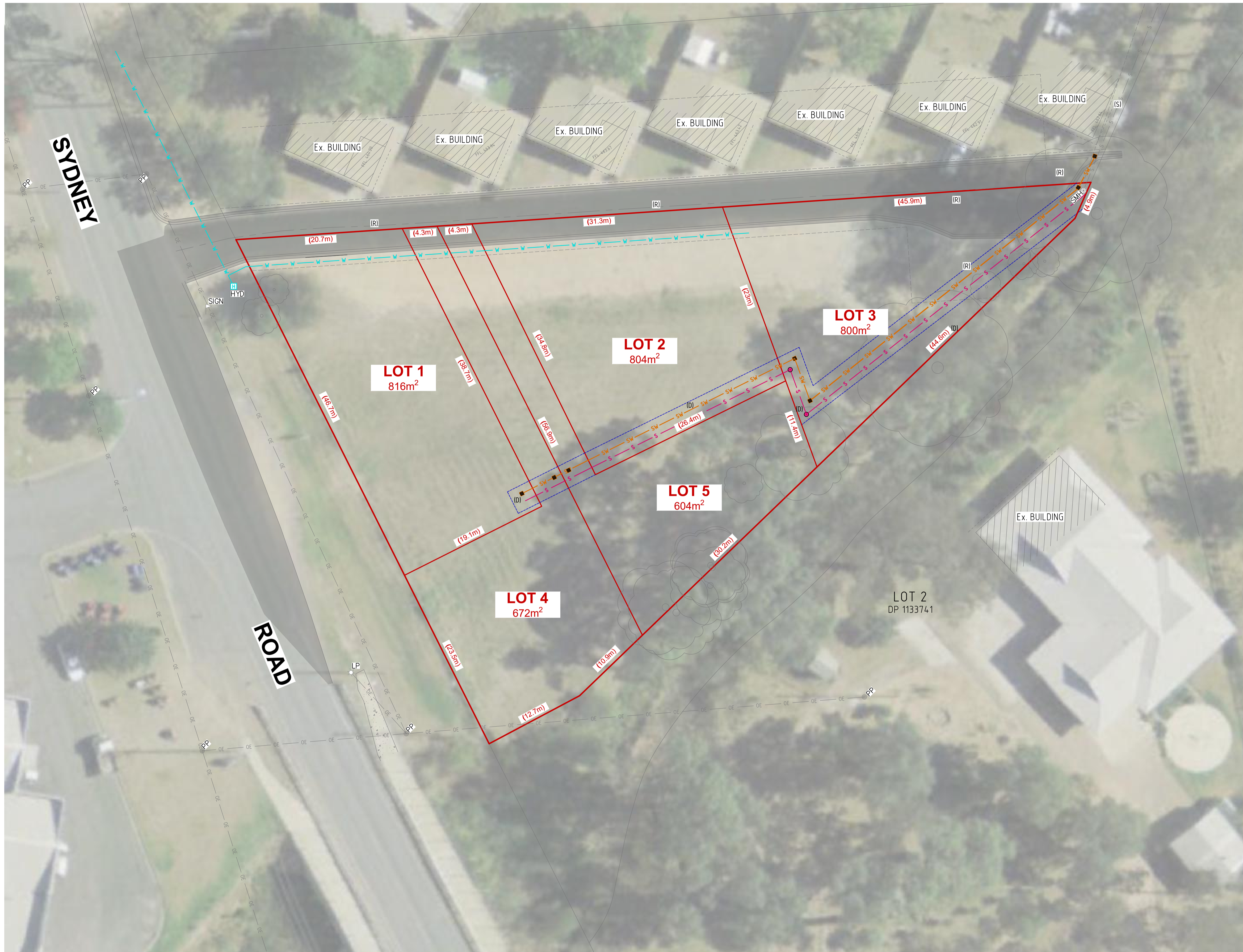
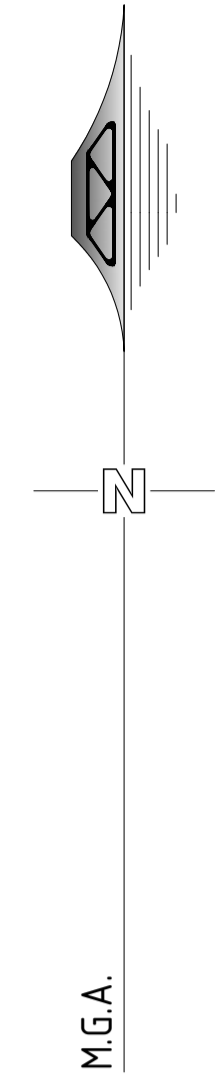


LEGEND	
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	EXISTING FENCE LINE
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	EXISTING UNDERGROUND SEWER PIPE
	EXISTING SEWER MANHOLE

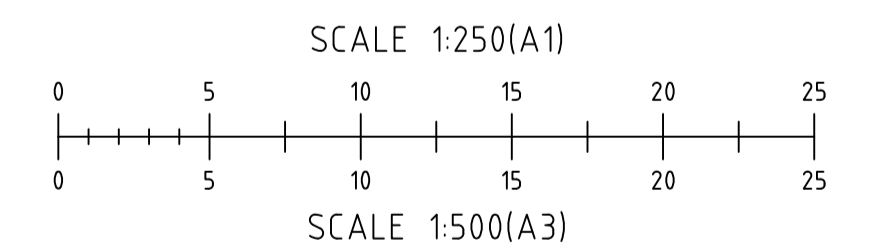
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 (S) EASEMENT TO DRAIN SEWAGE VARIABLE WIDTH (DP 1136929)

LEGEND (PROPOSED)	
	PROPOSED BOUNDARY
	PROPOSED EASEMENT BOUNDARY
	PROPOSED U/G WATER MAIN
	PROPOSED U/G SEWER MAIN
	PROPOSED U/G STORMWATER DRAINAGE PIPE
	PROPOSED SEWER MANHOLE
	PROPOSED STORMWATER INLET PIT

(DI) EASEMENT TO DRAIN SEWAGE & STORMWATER, 3m WIDE



PROPOSED SITE PLAN
 REDUCTION RATIO 1:250 @ A1
 1:500 @ A3



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Client: **ROBERT KENNEDY**
 Project: **PROPOSED SUBDIVISION OF 1B SYDNEY ROAD MUDGEE 2850**
 Drawing Title: **PROPOSED SITE PLAN**

Rev	Date	Amendment
A	16-06-2022	ISSUED FOR APPROVAL
B	6-09-2022	SERVICES AMENDED

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Drawn	MK		
Check	LM	Drawing Number	Revision
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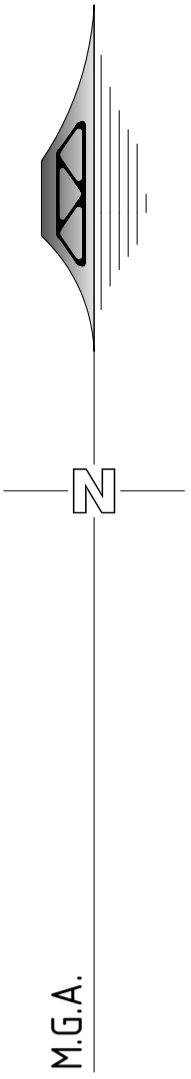


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	EXISTING LIGHT POLE
	EXISTING UNDERGROUND SEWER PIPE
	EXISTING SEWER MANHOLE

(R) RIGHT OF CARRIAGEWAY VARIABLE WIDTH (DP 1136929)
 (S) EASEMENT TO DRAIN SEWAGE VARIABLE WIDTH (DP 1136929)

LEGEND (PROPOSED)	
	PROPOSED BOUNDARY
	PROPOSED EASEMENT BOUNDARY

(D) EASEMENT TO DRAIN SEWAGE & STORMWATER, 3m WIDE

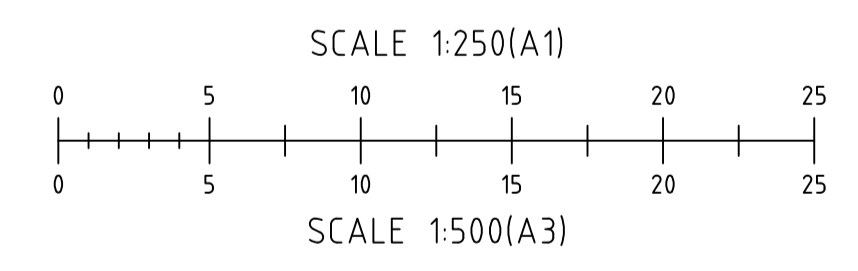


SYDNEY

ROAD



PROPOSED LOT PLAN
 REDUCTION RATIO 1:250 @ A1
 1:500 @ A3



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Client: **ROBERT KENNEDY**
 Project: **PROPOSED SUBDIVISION OF 1B SYDNEY ROAD MUDGEE 2850**
 Drawing Title: **PROPOSED LOT PLAN**

Rev	Date	Amendment
A	16-06-2022	ISSUED FOR APPROVAL
B	6-09-2022	SERVICES AMENDED

Design	LM	Certification	
Drawn	MK		
Check	LM	Drawing Number	Revision
Original Sheet Size = A1		38914 - C03	B



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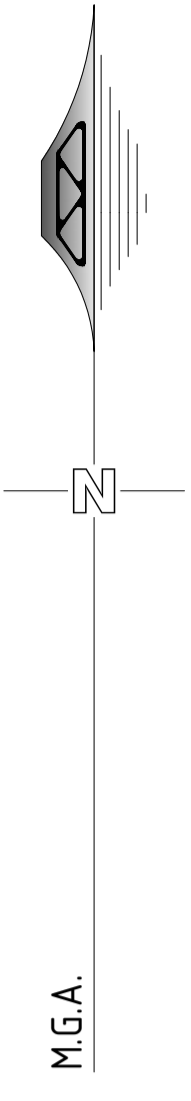
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	EXISTING LIGHT POLE
	EXISTING UNDERGROUND SEWER PIPE
	EXISTING SEWER MANHOLE

(R) RIGHT OF CARRIAGEWAY VARIABLE WIDTH (DP 1136929)
 (S) EASEMENT TO DRAIN SEWAGE VARIABLE WIDTH (DP 1136929)

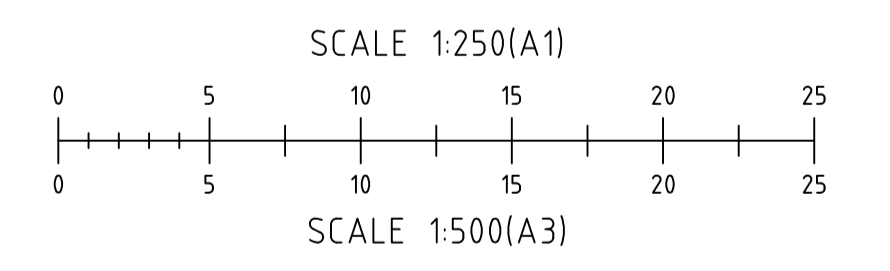
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	PROPOSED EASEMENT BOUNDARY
	PROPOSED U/G WATER MAIN
	PROPOSED U/G SEWER MAIN
	PROPOSED U/G STORMWATER DRAINAGE PIPE
	PROPOSED SEWER MANHOLE
	PROPOSED STORMWATER INLET PIT

(D) EASEMENT TO DRAIN SEWAGE & STORMWATER, 3m WIDE



PROPOSED STORMWATER PLAN
 REDUCTION RATIO 1:250 @ A1
 1:500 @ A3



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Client: **ROBERT KENNEDY**
 Project: **PROPOSED SUBDIVISION OF 1B SYDNEY ROAD MUDGEE 2850**
 Drawing Title: **PROPOSED STORMWATER PLAN**

Rev	Date	Amendment
A	16-06-2022	ISSUED FOR APPROVAL
B	6-09-2022	SERVICES AMENDED

Design	LM	Certification	
Drawn	MK		
Check	LM	Drawing Number	Revision
Original Sheet Size = A1		38914 - C04	B



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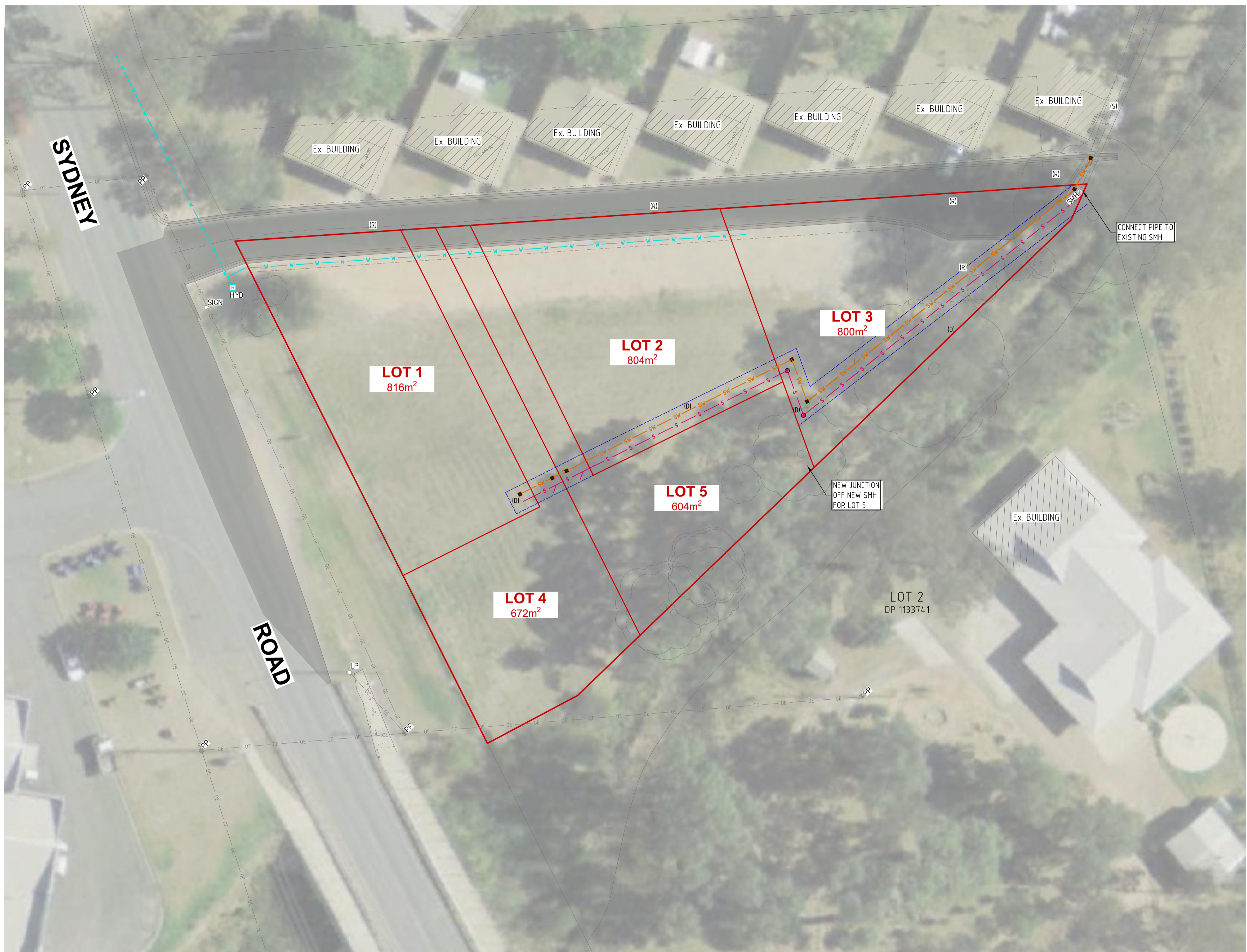
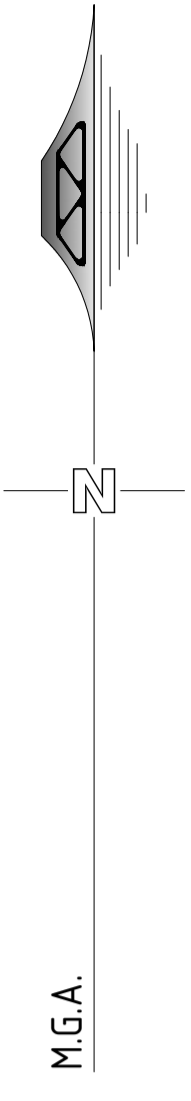
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	EXISTING LIGHT POLE
	EXISTING UNDERGROUND SEWER PIPE
	EXISTING SEWER MANHOLE

(R) RIGHT OF CARRIAGEWAY VARIABLE WIDTH (DP 1136929)
 (S) EASEMENT TO DRAIN SEWAGE VARIABLE WIDTH (DP 1136929)

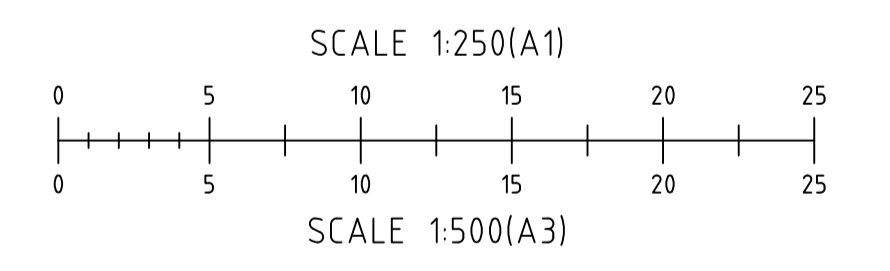
LEGEND (PROPOSED)

	PROPOSED BOUNDARY
	PROPOSED EASEMENT BOUNDARY
	PROPOSED U/G WATER MAIN
	PROPOSED U/G SEWER MAIN
	PROPOSED U/G STORMWATER DRAINAGE PIPE
	PROPOSED SEWER MANHOLE
	PROPOSED STORMWATER INLET PIT

(D) EASEMENT TO DRAIN SEWAGE & STORMWATER, 3m WIDE



PROPOSED SEWER PLAN
 REDUCTION RATIO 1:250 @ A1
 1:500 @ A3



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Client: **ROBERT KENNEDY**
 Project: **PROPOSED SUBDIVISION OF 1B SYDNEY ROAD MUDGEE 2850**
 Drawing Title: **PROPOSED SEWER PLAN**

Rev	Date	Amendment
A	16-06-2022	ISSUED FOR APPROVAL
B	6-09-2022	SERVICES AMENDED

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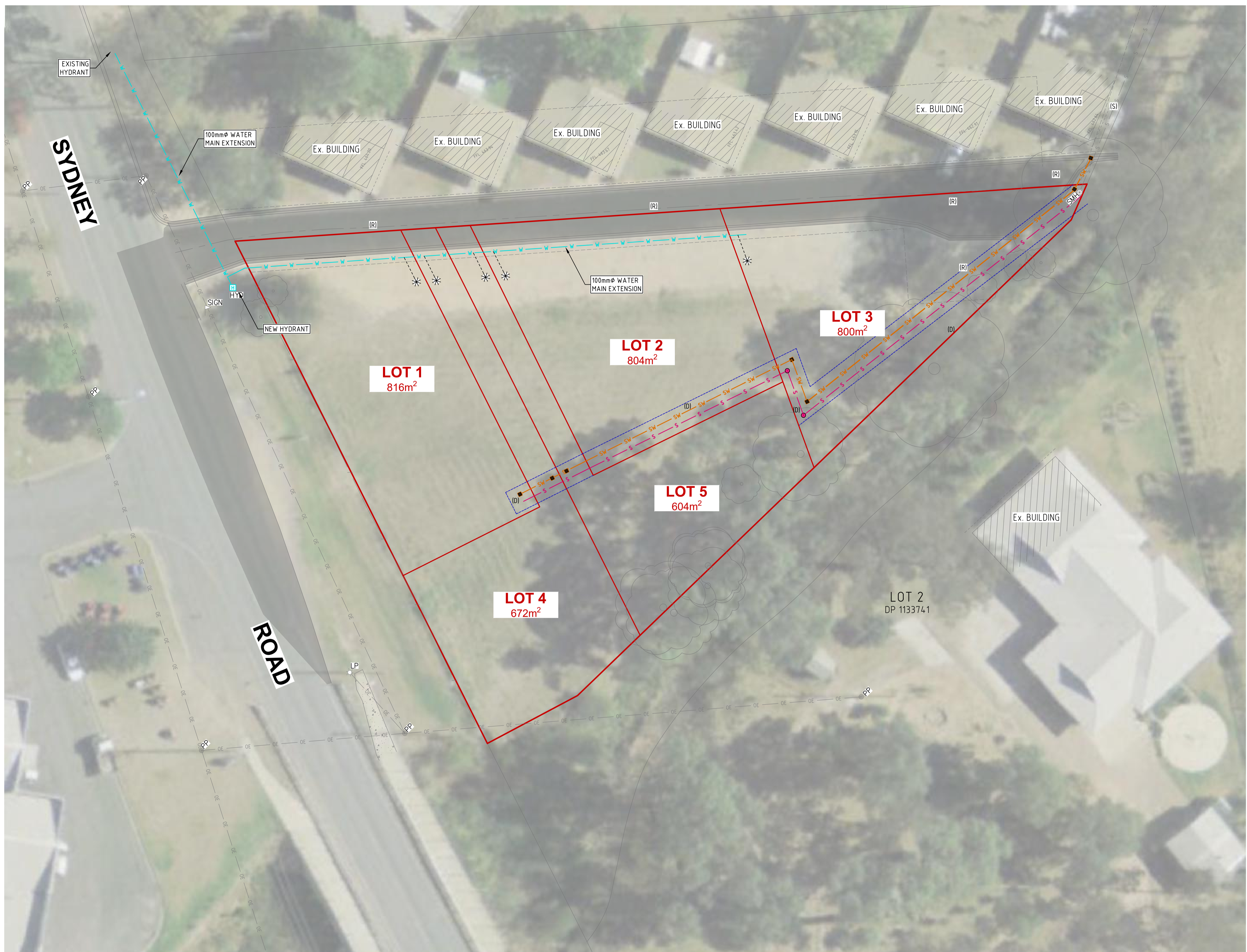
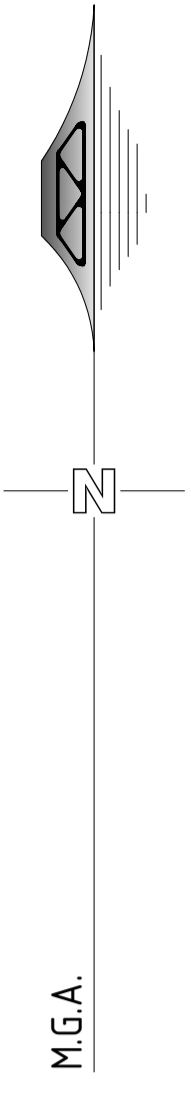
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- ⊙ PP EXISTING POWER POLE
- ⊙ LP EXISTING LIGHT POLE
- S - S - EXISTING UNDERGROUND SEWER PIPE
- ⊙ SMH EXISTING SEWER MANHOLE

- (R) RIGHT OF CARRIAGEWAY VARIABLE WIDTH (DP 1136929)
- (S) EASEMENT TO DRAIN SEWAGE VARIABLE WIDTH (DP 1136929)

LEGEND (PROPOSED)

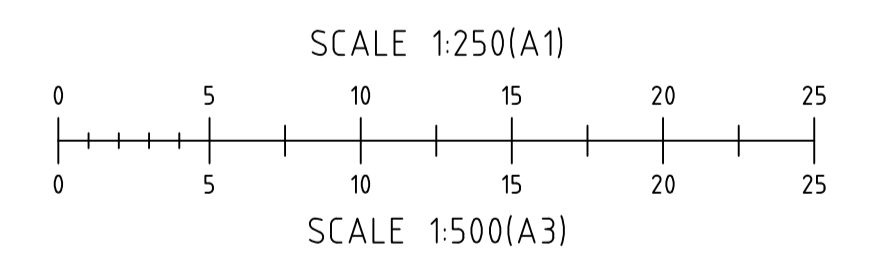
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- W - PROPOSED U/G WATER MAIN
- S - PROPOSED U/G SEWER MAIN
- SW - PROPOSED U/G STORMWATER DRAINAGE PIPE
- ⊙ PROPOSED SEWER MANHOLE
- ⊙ PROPOSED STORMWATER INLET PIT
- * PROPOSED WATER METER

- (D) EASEMENT TO DRAIN SEWAGE & STORMWATER, 3m WIDE



PROPOSED WATER PLAN

REDUCTION RATIO 1:250 @ A1
1:500 @ A3



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 Drawing Title: **PROPOSED WATER PLAN**

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Drawn	MK		
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Original Sheet Size = A1		38914 - C06	B

Appendix D - Traffic Impact Assessment



**TRAFFIC AND PARKING IMPACT ASSESSMENT OF
THE PROPOSED RESIDENTIAL SUBDIVISION
AT 1B SYDNEY ROAD, MUDGEE**



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Division of RAMTRANS Australia ABN: 45067491678 RPEQ: 19457

Transport Planning, Traffic Impact Assessments, Road Safety Audits, Expert Witness

Development Type: Residential Subdivision
Site Address: 1B Sydney Road, Mudgee
Prepared for: Barnson Pty Ltd
Document reference: 220536.01FA

Status	Issue	Prepared By	Checked By	Date
Draft	A	JC	AT	19 August 2022
Final	A	AT	LS	22 August 2022

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1 INTRODUCTION

McLaren Traffic Engineering was commissioned by Barnson Pty Ltd to provide a traffic and parking impact assessment of the proposed residential subdivision at 1B Sydney Road, Mudgee as depicted in **Annexure A**.

1.1 **Description and Scale of Development**

The proposed development has the following characteristics relevant to traffic and parking:

- Subdivision of 3,696m² of land into five (5) residential lots;
- All vehicular access via the existing Right of Way (RoW) along the northern boundary of the lane.

1.2 **State Environmental Planning Policy (Transport and Infrastructure) 2021**

The proposed development has frontage to a classified road and therefore qualifies as such with reference to *Clause 2.119 of SEPP (Transport and Infrastructure) 2021*. The development therefore must satisfy that:

a) where practicable and safe, vehicular access to the land is provided by a road other than the classified road, and

(b) the safety, efficiency, and ongoing operation of the classified road will not be adversely affected by the development as a result of:

(i) the design of the vehicular access to the land.

(ii) the emission of smoke or dust from the development

(iii) the nature, volume or frequency of vehicles using the classified road to gain access to the land.

The proposed site contains a frontage to Sydney Road (Castlereagh Highway), a TfNSW classified road and accordingly, the consent authority (Mid-Western Regional Council) must be satisfied that the proposed development satisfies Clause 2.119.

An assessment of the proposal against the criteria provided in Clause 2.119 of *SEPP (Transport and Infrastructure)* is undertaken in **Section 4.4**.

The proposed development does not qualify as a traffic generating development with relevant size and/or capacity under *Clause 2.122 of the SEPP (Transport and Infrastructure) 2021*. Accordingly, formal referral to Transport for NSW (TfNSW) is unnecessary and the application can be assessed by Mid-Western Regional Council officers accordingly.

1.3 Site Description

The subject development is currently zoned *R1 – General Residential* under the *Mid-Western Regional Local Environmental Plan 2012* and is currently a vacant lot. The site has frontages to Sydney Road to the east and an unnamed RoW to the north and is located in rural NSW.

The site is generally surrounded by industrial developments to the west and south, low to medium-density residential developments to the north and rural land to the east. Mudgee Golf Club is located directly to the south of the subject site, with the Mudgee town centre located approximately 1.3km to the northwest.

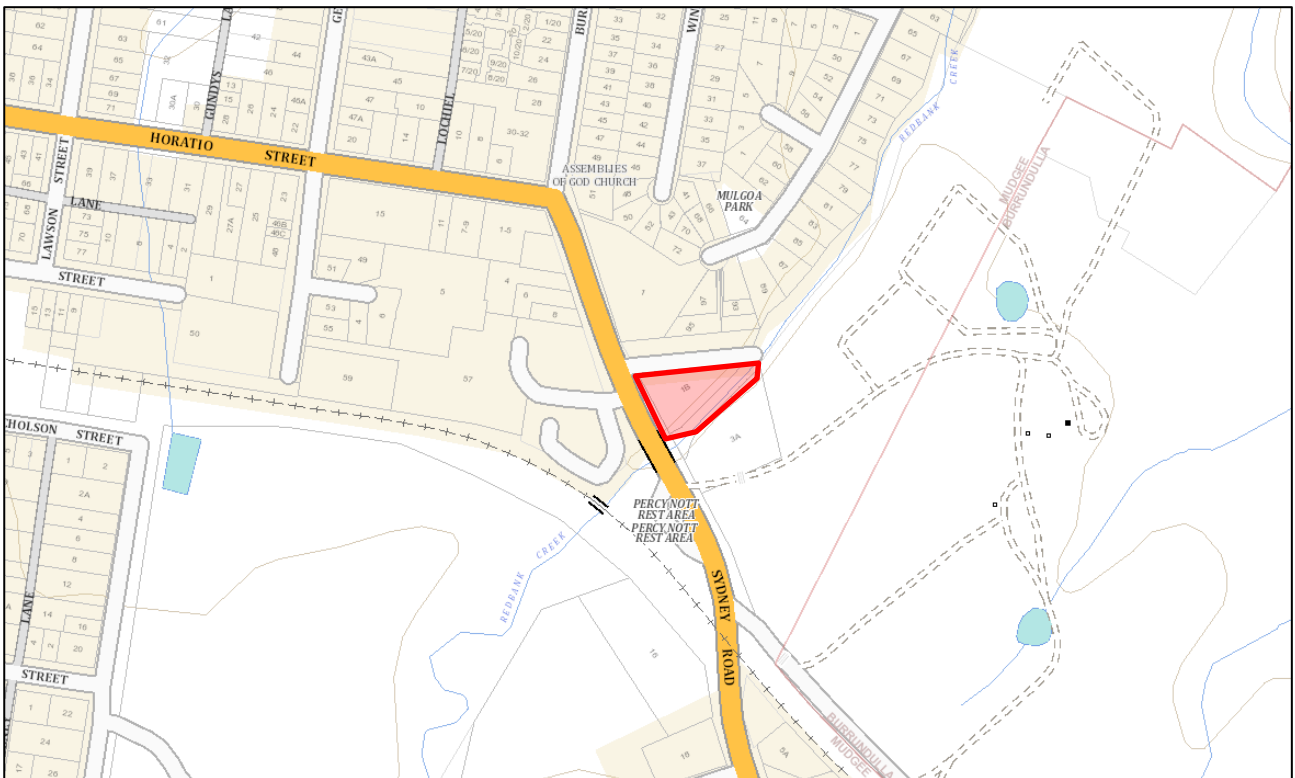
1.4 Site Context

The location of the site is shown on an aerial photo and a street map in **Figure 1** and **Figure 2** respectively.



 Site Location

FIGURE 1: SITE CONTEXT – AERIAL PHOTO



 Site Location

FIGURE 2: SITE CONTEXT – STREET MAP

2 EXISTING TRAFFIC AND PARKING CONDITIONS

2.1 Road Hierarchy

The road network servicing the site has characteristics as described in the following sub-sections.

2.1.1 Sydney Road / Castlereagh Highway

- RMS Classified STATE Highway (No. 18);
- Approximately 13m wide two-way carriageway facilitating one (1) traffic flow lane in each direction and linemarked kerbside parking & sealed road shoulder on both sides of the road;
- Signposted 50km/h speed limit;
- Generally, unrestricted kerbside parking permitted along both sides of the road to the north of the site, with a linemarked road shoulder to the south of the site.

2.2 Existing Traffic Management

- Signposted “GIVE-WAY” intersection of Burrundulla Avenue / Sydney Road:
 - Signposted “NO RIGHT TURN” restriction from Burrundulla Avenue;
 - Concrete central median restricting right turns from Sydney Road into Burrundulla Avenue.
- Priority controlled driveway at 1A/1B Sydney Road Right of Way (RoW) / Sydney Road (subject property access).

2.3 Existing Traffic Environment

Turning movement count surveys were conducted at the intersection of the subject site’s RoW / Sydney Road / Industrial Access Driveway from 6:30 AM – 9:30 AM and 2:30 PM – 6:30 PM on Wednesday 3 March 2021, representing a typical operating weekday. The full survey results are shown in **Annexure B** for reference.

2.3.1 Existing Road Performance

The performance of the surrounding intersections under the existing traffic conditions has been assessed using SIDRA INTERSECTION 9.0, **Table 1** summarises the resultant intersection performance data, with full SIDRA results reproduced in **Annexure C**.

TABLE 1: EXISTING INTERSECTION PERFORMANCES (SIDRA INTERSECTION 9.0)

Intersection	Peak Hour	Degree of Saturation ⁽¹⁾	Average Delay ⁽²⁾ (sec/veh)	Level of Service ⁽³⁾⁽⁴⁾	Control Type	Worst Movement
EXISTING PERFORMANCE						
Sydney Road / Right of Way	AM	0.33	0.2 (Worst: 18.8)	NA (Worst: C)	Give Way	RT from Right of Way
	PM	0.36	0.1 (Worst: 21.6)	NA (Worst: C)		RT from Right of Way

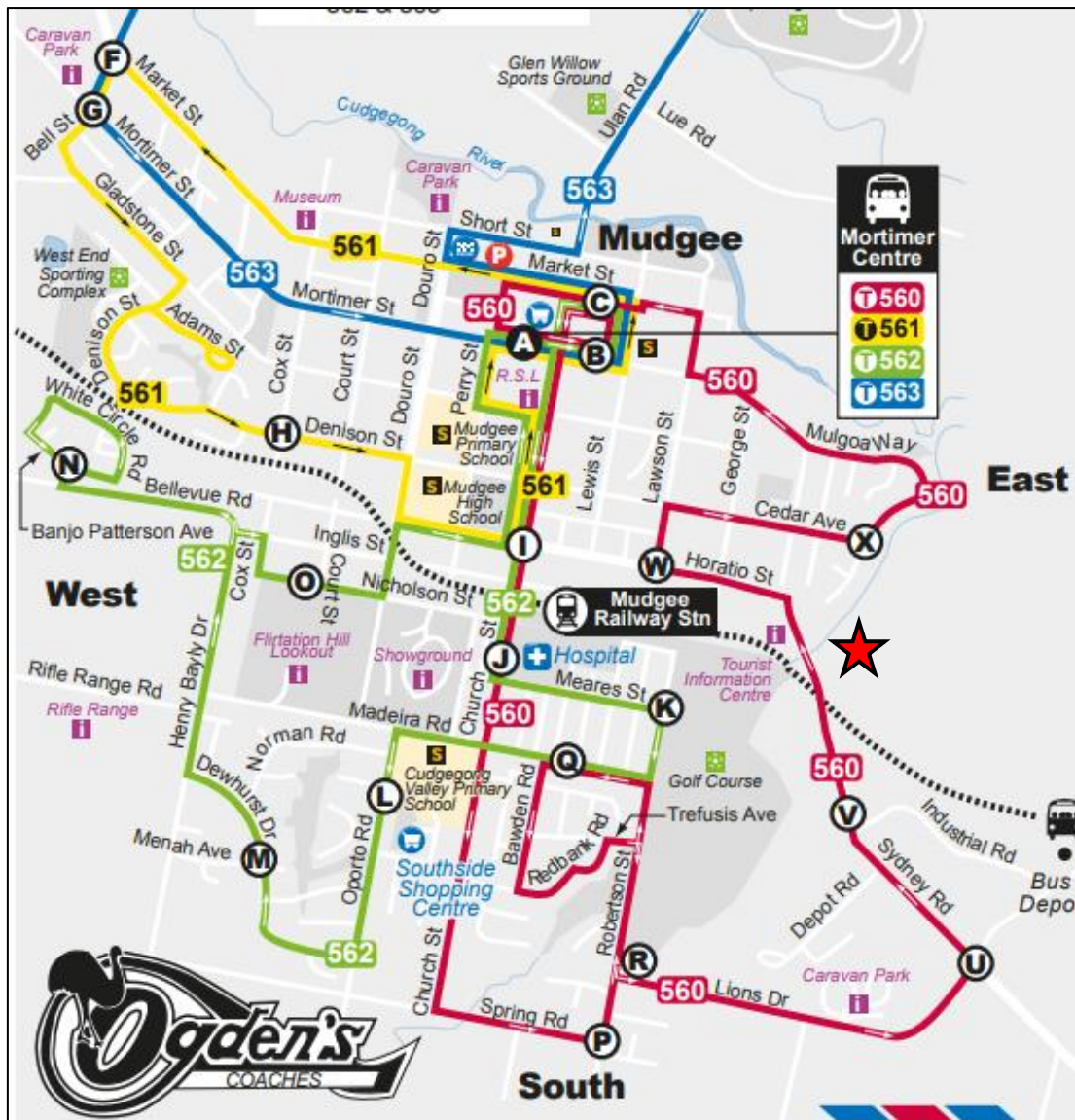
Notes:

- (1) The Degree of Saturation is the ratio of demand to capacity for the most disadvantaged movement.
- (2) The average delay is the delay experienced on average by all vehicles. The value in brackets represents the delay to the most disadvantaged movement.
- (3) The Level of Service is a qualitative measure of performance describing operational conditions. There are six levels of service, designated from A to F, with A representing the best operational condition and level of service F the worst. The LoS of the intersection is shown in bold, and the LoS of the most disadvantaged movement is shown in brackets.
- (4) No overall Level of Service is provided for Give Way and Stop controlled intersections as the low delays associated with the dominant movements skew the average delay of the intersection. The Level of Service of the worst approach is an indicator of the operation of the intersection, with a worse Level of Service corresponding to long delays and reduced safety outcomes for that approach.

As shown above, the Right of Way driveway with Sydney Road is currently performing at a satisfactory level of efficiency, with a worst movement Level of Service “C” condition during both the AM and PM peak periods. The worst movement Level of Service “C” performance is characterised by satisfactory approach delays and spare capacity.

2.4 Public Transport

The subject site is located in rural NSW and has limited access to public transport services. The nearest bus stop (ID: 285016) is located approximately 600m walking distance from the site which services bus route 560 (Mudgee to Mudgee East) provided by Ogden’s Coaches. It is noted that Mudgee Railway Station is no longer in operation. A map of the public transport network surrounding the site is depicted in **Figure 3**.



 Site Location

FIGURE 3: PUBLIC TRANSPORT NETWORK MAP

2.5 Future Road and Infrastructure Upgrades

From Mid-Western Regional Council Development Application tracker and website, it appears that there are no future planned road or public transport changes that will affect traffic conditions within the immediate vicinity of the subject site.

3 PARKING ASSESSMENT

3.1 Council Parking Requirement

Reference is made to the *Mid-Western Regional Council Development Control Plan 2013 - Part 5: Development Standards* which designates the following parking rates applicable to the proposed development:

5.1 – Car Parking

Dwellings (including dual occupancy)

2 spaces per dwelling – 1 space to be a garage, 1 space may be provided in a stacked arrangement in front of the garage provided that the space is contained wholly within the site

Table 2 presents the parking requirements of the proposal according to the Council’s above car parking rates.

TABLE 2: DCP CAR PARKING RATES

Land Use	Scale	Rate	Spaces Required
Residential Subdivision	5 residential lots	2 per dwelling	10

As shown, strict application of the DCP requires the provision of **10** car parking spaces (2 per dwelling). Whilst the floor plan and design of any residential development on each lot is not part of this DA, it is expected that each lot has adequate area to accommodate the car parking requirements of the Mid-Western Regional Council DCP 2013. The car parking provision of each residential development will be assessed at the DA stage for those developments.

3.2 Bicycle & Motorcycle Parking Requirements

The Mid-Western Regional Council DCP 2013 does not require the provision of bicycle / motorcycle parking. No bicycle / motorcycle has been provided, satisfying Council requirements. It is expected that there will be sufficient storage in each enclosed garage associated with each dwelling to be able to store a bicycle or motorcycle if necessary.

3.3 Servicing & Loading

It is expected that servicing and loading will be conducted along either the existing RoW or utilising the existing linemarked kerbside parking along Sydney Road. Similarly, it is expected that garbage collection will be conducted along Sydney Road similar to the existing operations of neighbouring sites (i.e. 1A Sydney Road, Mudgee).

3.4 Subdivision Design

All lots within the proposed subdivision obtain vehicular access from the existing Right of Way (RoW) along the northern boundary of the site. No new roads are proposed as part of this subdivision. The existing RoW has sufficient width to accommodate the access arrangements of the subject five (5) lot subdivision. Two lots are provided as battleaxe lots, and are provided with a 4m wide access handle, meeting the requirements outlined in Part 7.1 of the Mid-Western Regional DCP 2013

Both residual lots that retain a frontage to Sydney Road, should only seek access via the RoW. Direct vehicular access to Sydney Road should be avoided.

4 TRAFFIC ASSESSMENT

The impact of the expected traffic generation levels associated with the subject proposal is discussed in the following sub-sections.

4.1 **Traffic Generation**

Traffic generation rates for the relevant land uses are provided in the *RTA Guide to Traffic Generating Developments (2002)* and recent supplements as adopted by Transport for NSW (TfNSW) and are as follows:

TDT 2013/04a

Low density residential dwellings

Weekday average evening peak hour vehicle trips = 0.99 per dwelling in Sydney (maximum 1.39), 0.78 per dwelling in regional areas (maximum 0.90).

Weekday average morning peak hour vehicle trips = 0.95 per dwelling in Sydney (maximum 1.32), 0.71 per dwelling in regional areas (maximum 0.85).

The resulting AM and PM peak hourly traffic generation is summarised in **Table 3**.

TABLE 3: ESTIMATED TRAFFIC GENERATION

Use	Scale	Peak	Generation Rate	Trips ⁽¹⁾
Low-Density Residential	5 Dwellings	AM	0.71 per dwelling	4 (1 in, 3 out)
		PM	0.78 per dwelling	4 (3 in, 1 out)

Note:

(1) Assumes 20% inbound & 80% outbound during AM peak and vice versa for PM peak.

As shown, the expected traffic generation associated with the proposed development is in the order of four (4) vehicle trips in the AM peak period (1 in, 3 out) and four (4) vehicle trips in the PM peak period (3 in, 1 out).

4.2 **Traffic Assignment**

The road network, traffic surveys and locations of residential areas surrounding the site have been assessed and the following traffic assignment has been assumed for all traffic to and from the site:

- 50% to / from the north via Sydney Road;
- 50% to / from the south via Sydney Road;

4.3 Traffic Impact

The traffic generation outlined in **Section 4.1 & 4.2** above has been added to the existing traffic volumes recorded. SIDRA INTERSECTION 9.0 was used to assess the intersections performance. The purpose of this assessment is to compare the existing intersection operations to the future scenario under the increased traffic load. The results of this assessment are shown in **Table 4**.

TABLE 4: INTERSECTION PERFORMANCE (SIDRA INTERSECTION 9.0)

Intersection	Peak Hour	Degree of Saturation ⁽¹⁾	Average Delay ⁽²⁾ (sec/veh)	Level of Service ⁽³⁾⁽⁴⁾	Control Type	Worst Movement
EXISTING PERFORMANCE						
Sydney Road / Right of Way	AM	0.33	0.2 (Worst: 18.8)	NA (Worst: C)	Give Way	RT from Right of Way
	PM	0.36	0.1 (Worst: 21.6)	NA (Worst: C)		RT from Right of Way
FUTURE (POST DEVELOPMENT) PERFORMANCE						
Sydney Road / Right of Way	AM	0.33	0.3 (Worst: 19.2)	NA (Worst: C)	Give Way	RT from Right of Way
	PM	0.36	0.1 (Worst: 22)	NA (Worst: C)		RT from Right of Way

Notes: Refer to Table 1.

As shown, the junction of the Right of Way driveway with Sydney Road retains the same worst movement Level of Service under future conditions with minimal delays and additional capacity, indicating that there will be negligible impact on the existing road network as a result of the proposed development.

4.4 SEPP (Transport and Infrastructure) Clause 2.119

The proposed development has frontage to Sydney Road (Castlereagh Highway), a TfNSW Classified Highway (No. 18) and as such, an assessment against the criteria in *Clause 2.119 of SEPP (Transport and Infrastructure)* is presented below. The relevant items raised in Clause 2.119 are presented below (italicised) with MTE response thereafter.

(a) where practicable and safe, vehicular access to the land is provided by a road other than the classified road, and

MTE Response: The subject site has a single frontage to Sydney Road (Castlereagh Highway), therefore, no alternate vehicular access is possible to the public road network. All residential lots achieve vehicular access to Sydney Road from an existing RoW along the northern side of the site, such that no additional driveways to Sydney Road are created, achieving the best possible outcome.

(b) the safety, efficiency, and ongoing operation of the classified road will not be adversely affected by the development as a result of:

i. the design of the vehicular access to the land.

MTE Response: Vehicular access to Sydney Road is via an existing RoW, there are no proposed changes to the RoW's access with Sydney Road. This RoW operates efficiently and there are clear sightlines in both directions such that no safety issues arise from the RoW.

ii. the emission of smoke or dust from the development

MTE Response: This is for others to address.

iii. the nature, volume or frequency of vehicles using the classified road to gain access to the land.

MTE Response: **Section 4** outlines the expected peak hour traffic generation and impact on the surrounding intersections. The traffic generation of the site will have negligible impact on the surrounding intersections.

5 CONCLUSION

In view of the foregoing, the subject residential subdivision proposal at 1B Sydney Road, Mudgee (as depicted in **Annexure A**) is fully supportable in terms of its traffic and parking impacts. The following outcomes of this traffic and parking impact assessment are relevant to note:

- Floor plans of each lot have not been produced, each lot is expected to have adequate area to accommodate the car parking requirement of the Mid-Western Regional Council 2013 and will be assessed for compliance as part of separate development applications.
- Council's DCP does not require the provision of bicycle and motorcycle parking facilities.
- The traffic generation of the proposed development has been estimated to be some 4 trips in the AM peak period (1 in, 3 out) and 4 trips in the PM peak period (3 in, 1 out). The impacts of the traffic generation have been modelled using SIDRA INTERSECTION 9.0, indicating that there will be no detrimental impact to the performance of the intersections as a result of the generated traffic.

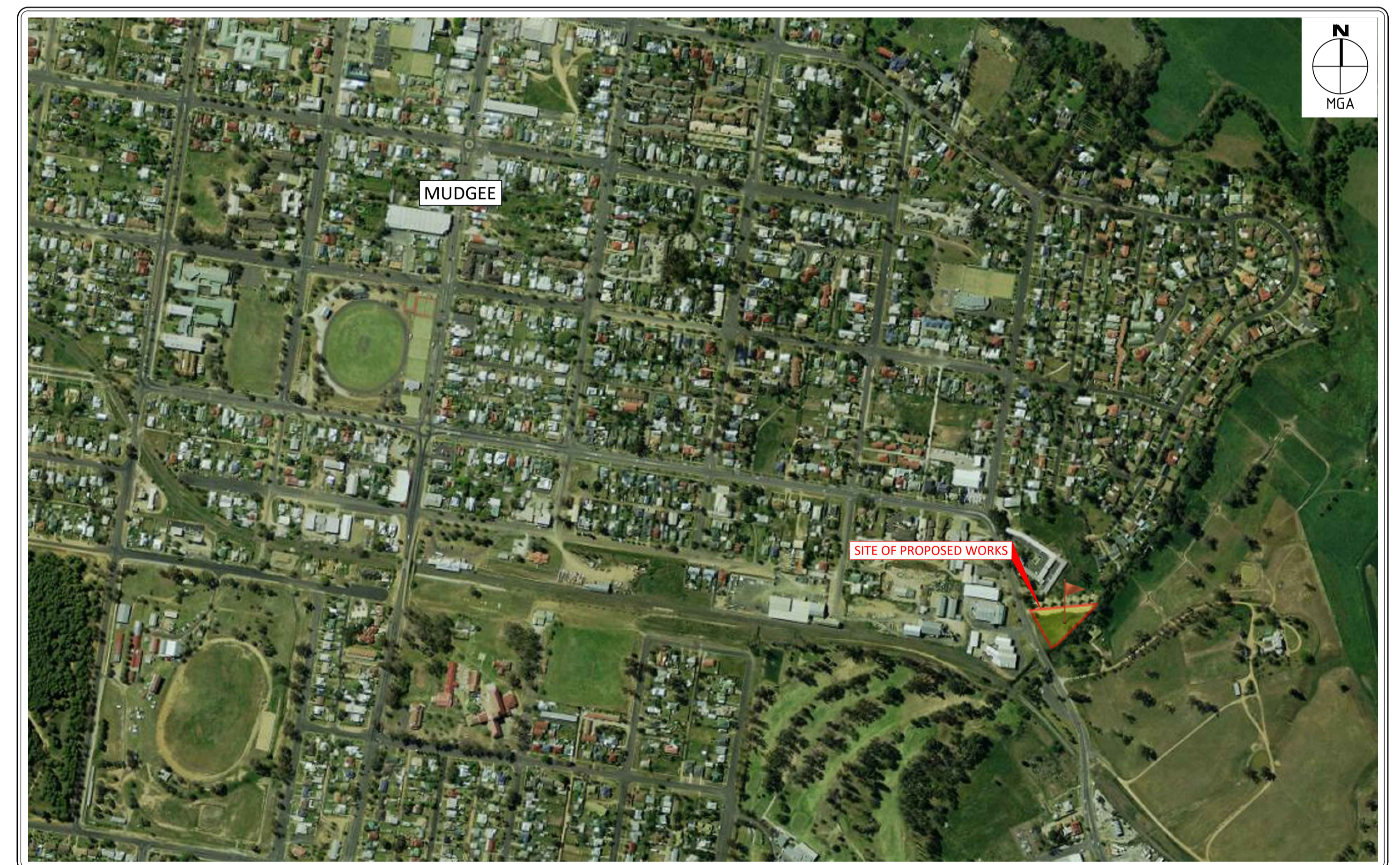


**ANNEXURE A: PROPOSED PLANS
(7 SHEETS)**

Development Application Documentation for the Proposed Subdivision of 1B Sydney Road, Mudgee, NSW 2850

SCHEDULE OF DRAWINGS

SHEET No.	DESCRIPTION
34914-C00	CIVIL ENGINEERING COVER SHEET
34914-C01	EXISTING SITE PLAN
34914-C02	PROPOSED SITE PLAN
34914-C03	PROPOSED LOT PLAN
34914-C04	PROPOSED STORMWATER PLAN
34914-C05	PROPOSED SEWER PLAN
34914-C06	PROPOSED WATER PLAN



LOCALITY PLAN
NOT TO REDUCTION RATIO

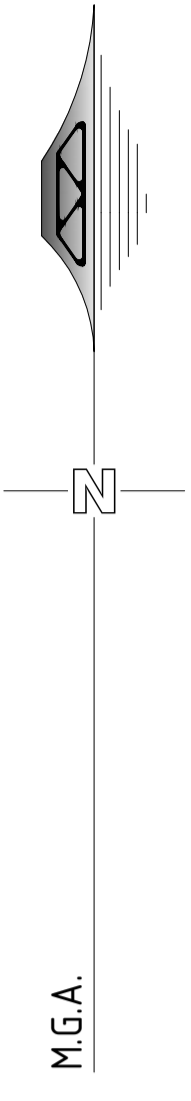
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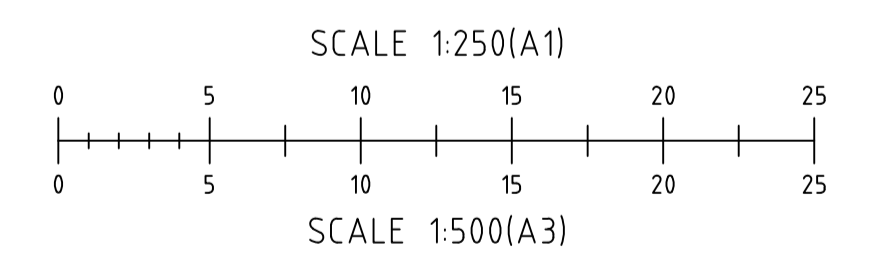
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- ⊙ SMH EXISTING SEWER MANHOLE

(R) RIGHT OF CARRIAGEWAY VARIABLE WIDTH (DP 1136929)
 (S) EASEMENT TO DRAIN SEWAGE VARIABLE WIDTH (DP 1136929)



EXISTING SITE PLAN
 REDUCTION RATIO 1:250 @ A1
 1:500 @ A3



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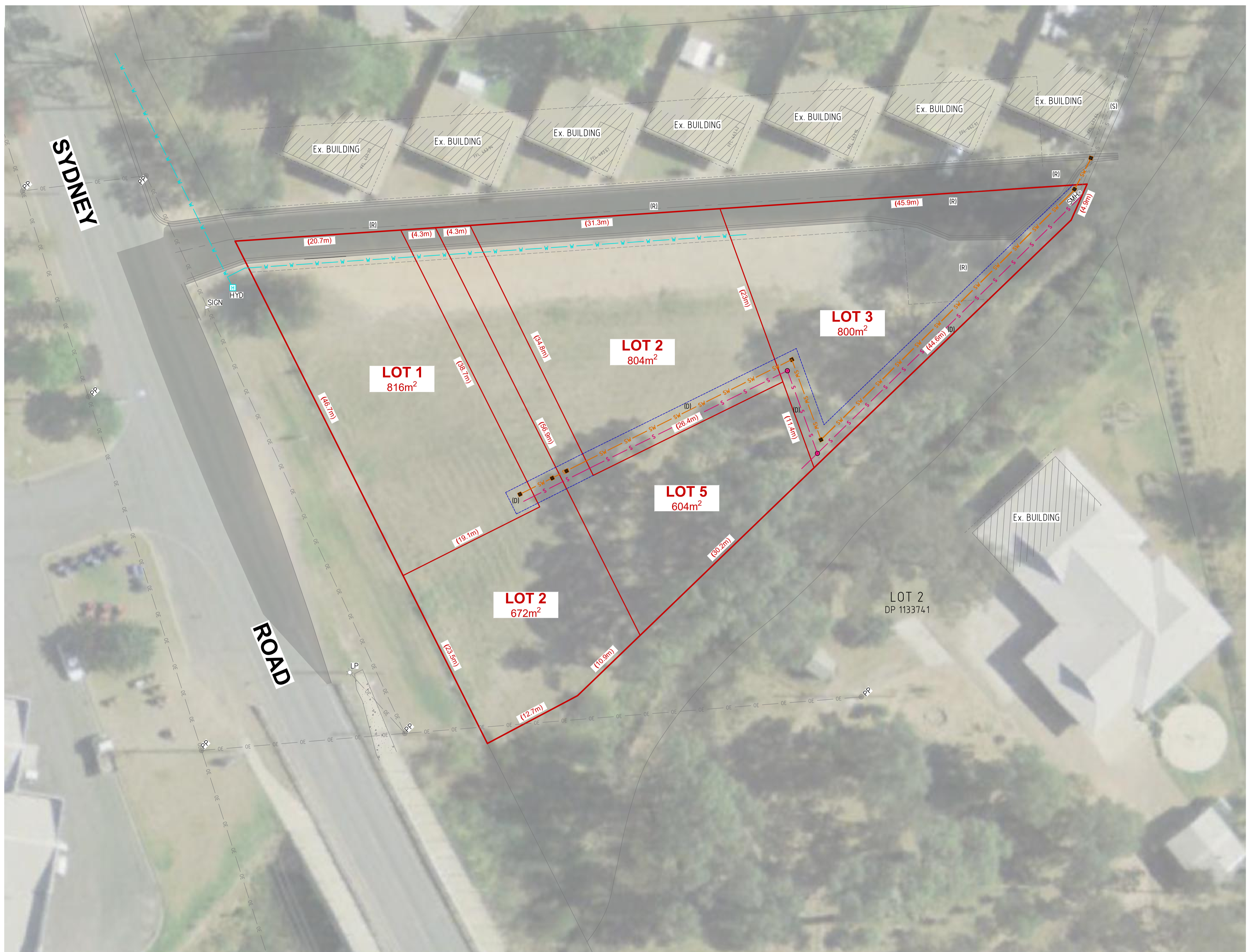
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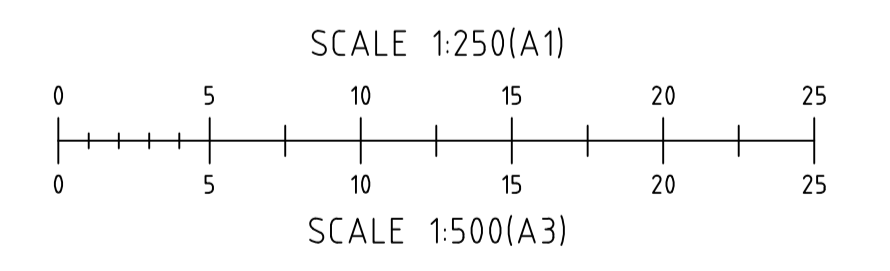
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- ⊙ PROPOSED STORMWATER INLET PIT

(DI) EASEMENT TO DRAIN SEWAGE & STORMWATER, 3m WIDE



PROPOSED SITE PLAN
 REDUCTION RATIO 1:250 @ A1
 1:500 @ A3



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 Drawing Title: **PROPOSED SITE PLAN**

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A	16-06-2022	ISSUED FOR APPROVAL

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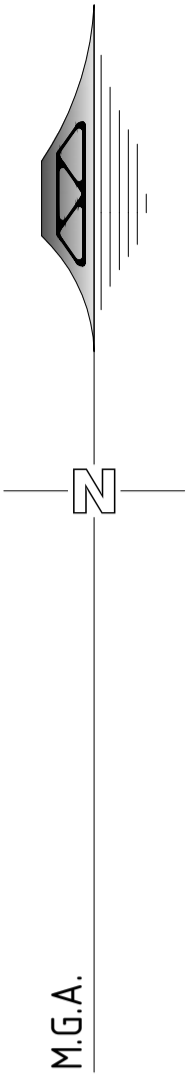


LEGEND	
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LEGEND (PROPOSED)	
	PROPOSED BOUNDARY
	PROPOSED EASEMENT BOUNDARY

(D) EASEMENT TO DRAIN SEWAGE & STORMWATER, 3m WIDE

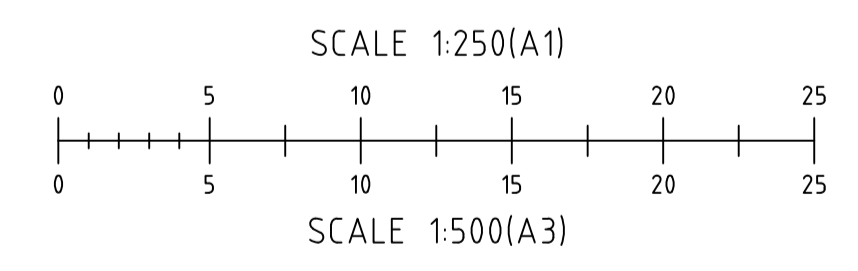


SYDNEY

ROAD



PROPOSED LOT PLAN
 REDUCTION RATIO 1:250 @ A1
 1:500 @ A3



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 Drawing Title: **PROPOSED LOT PLAN**

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Drawn	MK		
Check	LM	Drawing Number	Revision
Original Sheet Size = A1		38914 - C03	A



LEGEND

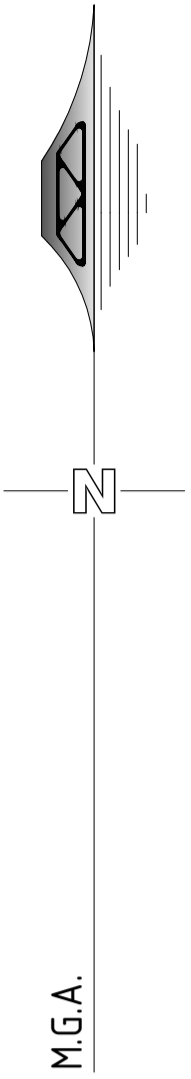
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- - - EXISTING EASEMENT
- / - EXISTING FENCE LINE
- OE — OE — EXISTING OVERHEAD ELECTRICITY LINE
- ⊙ PP EXISTING POWER POLE
- ⊙ LP EXISTING LIGHT POLE
- S — S — EXISTING UNDERGROUND SEWER PIPE
- ⊙ SMH EXISTING SEWER MANHOLE

(R) RIGHT OF CARRIAGEWAY VARIABLE WIDTH (DP 1136929)
 (S) EASEMENT TO DRAIN SEWAGE VARIABLE WIDTH (DP 1136929)

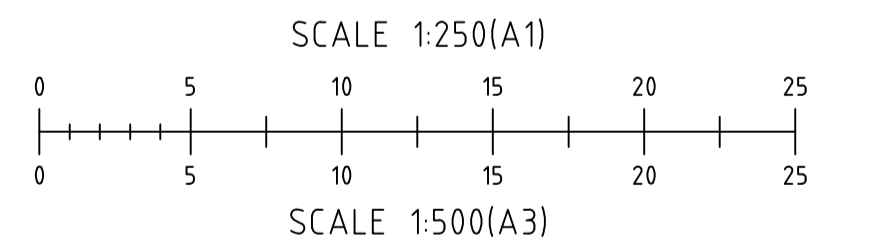
LEGEND (PROPOSED)

- - - PROPOSED BOUNDARY
- - - PROPOSED EASEMENT BOUNDARY
- W — PROPOSED U/G WATER MAIN
- S — PROPOSED U/G SEWER MAIN
- SW — PROPOSED U/G STORMWATER DRAINAGE PIPE
- ⊙ PROPOSED SEWER MANHOLE
- ⊙ PROPOSED STORMWATER INLET PIT

(D) EASEMENT TO DRAIN SEWAGE & STORMWATER, 3m WIDE



PROPOSED STORMWATER PLAN
 REDUCTION RATIO 1:250 @ A1
 1:500 @ A3



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 Project: **PROPOSED SUBDIVISION OF 1B SYDNEY ROAD MUDGEE 2850**
 Drawing Title: **PROPOSED STORMWATER PLAN**

Rev	Date	Amendment
A	16-06-2022	ISSUED FOR APPROVAL

Design	LM	Certification	
Drawn	MK		
Check	LM	Drawing Number	Revision
Original Sheet Size = A1		38914 - C04	A



LEGEND

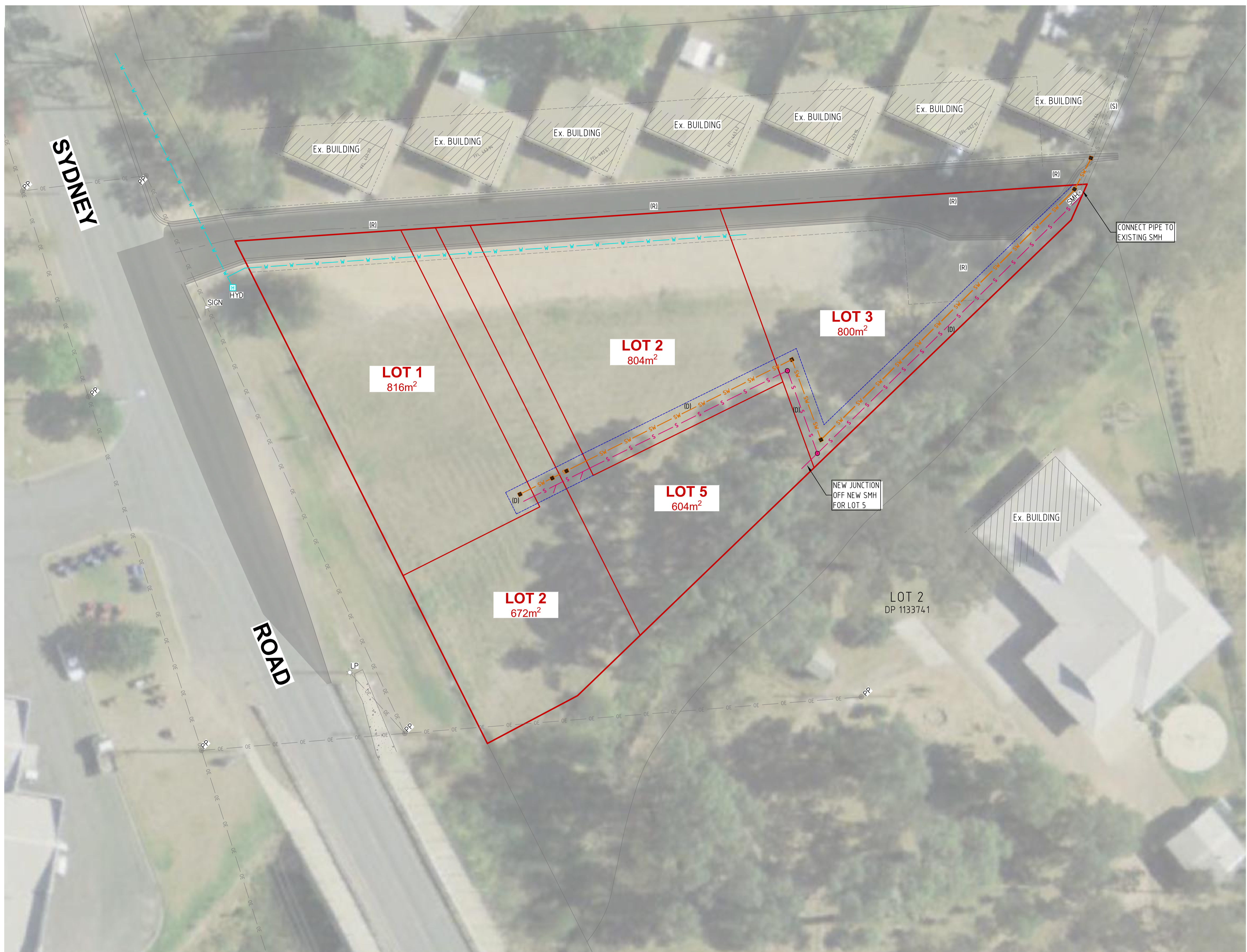
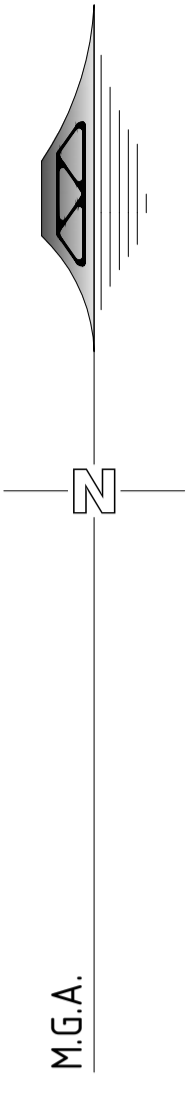
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- EXISTING EASEMENT
- EXISTING FENCE LINE
- EXISTING OVERHEAD ELECTRICITY LINE
- EXISTING POWER POLE
- EXISTING LIGHT POLE
- EXISTING UNDERGROUND SEWER PIPE
- EXISTING SEWER MANHOLE

- (R) RIGHT OF CARRIAGEWAY VARIABLE WIDTH (DP 1136929)
- (S) EASEMENT TO DRAIN SEWAGE VARIABLE WIDTH (DP 1136929)

LEGEND (PROPOSED)

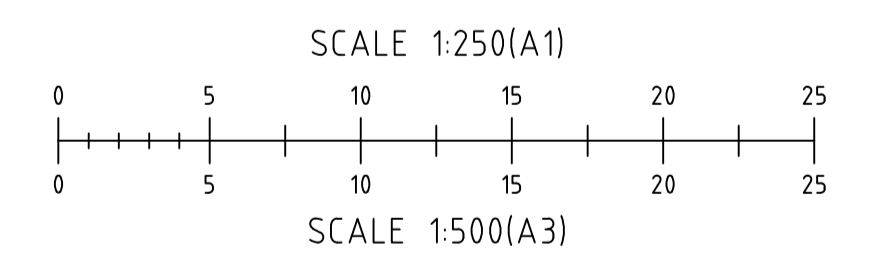
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- PROPOSED EASEMENT BOUNDARY
- PROPOSED U/G WATER MAIN
- PROPOSED U/G SEWER MAIN
- PROPOSED U/G STORMWATER DRAINAGE PIPE
- PROPOSED SEWER MANHOLE
- PROPOSED STORMWATER INLET PIT

- (DI) EASEMENT TO DRAIN SEWAGE & STORMWATER, 3m WIDE



PROPOSED SEWER PLAN

REDUCTION RATIO 1:250 @ A1
1:500 @ A3



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Client: **ROBERT KENNEDY**
 Project: **PROPOSED SUBDIVISION OF 1B SYDNEY ROAD MUDGEE 2850**
 Drawing Title: **PROPOSED SEWER PLAN**

Rev	Date	Amendment
A	16-06-2022	ISSUED FOR APPROVAL

Design	LM	Certification	
Drawn	MK		
Check	LM	Drawing Number	Revision
Original Sheet Size = A1		38914 - C05	A



LEGEND

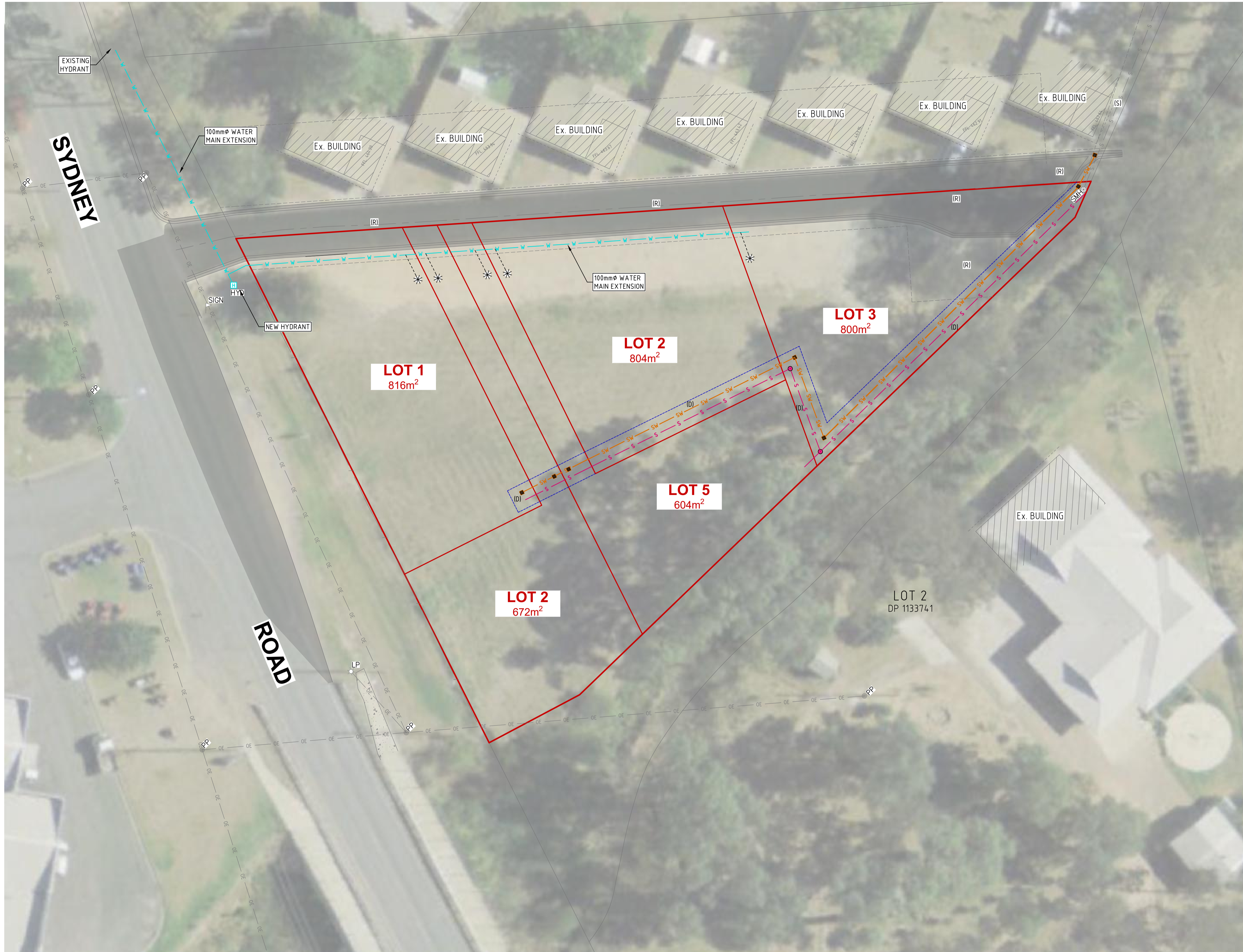
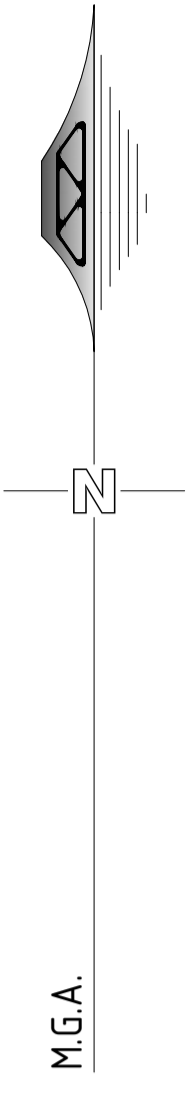
- EXISTING SUBJECT CADASTRAL BOUNDARIES
- - - - - EXISTING EASEMENT
- / - EXISTING FENCE LINE
- OE — OE — EXISTING OVERHEAD ELECTRICITY LINE
- ⊙ PP EXISTING POWER POLE
- ⊙ LP EXISTING LIGHT POLE
- S — S — EXISTING UNDERGROUND SEWER PIPE
- ⊙ SMH EXISTING SEWER MANHOLE

(R) RIGHT OF CARRIAGEWAY VARIABLE WIDTH (DP 1136929)
 (S) EASEMENT TO DRAIN SEWAGE VARIABLE WIDTH (DP 1136929)

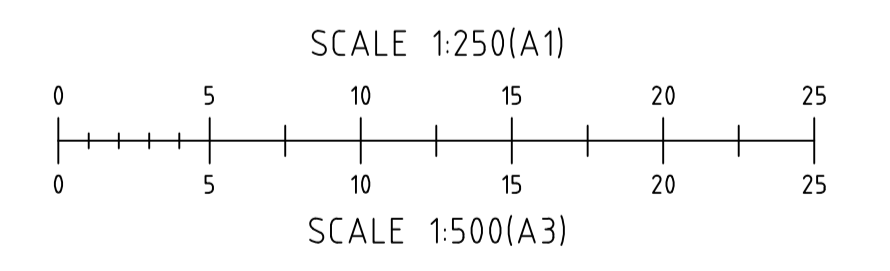
LEGEND (PROPOSED)

- - - PROPOSED BOUNDARY
- - - - - PROPOSED EASEMENT BOUNDARY
- W — PROPOSED U/G WATER MAIN
- S — PROPOSED U/G SEWER MAIN
- SW — PROPOSED U/G STORMWATER DRAINAGE PIPE
- ⊙ PROPOSED SEWER MANHOLE
- ⊙ PROPOSED STORMWATER INLET PIT
- * PROPOSED WATER METER

(DI) EASEMENT TO DRAIN SEWAGE & STORMWATER, 3m WIDE



PROPOSED WATER PLAN
 REDUCTION RATIO 1:250 @ A1
 1:500 @ A3



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Client: **ROBERT KENNEDY**
 Project: **PROPOSED SUBDIVISION OF 1B SYDNEY ROAD MUDGEE 2850**
 Drawing Title: **PROPOSED WATER PLAN**

Rev	Date	Amendment
A	16-06-2022	ISSUED FOR APPROVAL

Design	LM	Certification	
Drawn	MK		
Check	LM	Drawing Number	Revision
Original Sheet Size = A1		38914 - C06	A



**ANNEXURE B: TRAFFIC SURVEY DATA
(1 SHEET)**

TRANS TRAFFIC SURVEY

TURNING MOVEMENT SURVEY trafficsurvey.com.au



Intersection of ROW and Sydney Rd, Mudgee

GPS	-32.608223, 148.537361	North:	Sydney Rd	Survey	AM: 6:30 AM-9:30 AM
Date:	Wed 03/03/21	East:	ROW	PM:	2:30 PM-6:30 PM
Weather:	Overcast	South:	Sydney Rd	Traffic	AM: 8:30 AM-9:30 AM
Suburban:	Mudgee	West:	Industrial Service Rd	PM:	3:45 PM-4:45 PM
Customs:	McLaren				

Time		North Approach Sydney Rd				East Approach ROW				South Approach Sydney Rd				West Approach Industrial Service				Hourly Total	
Period Start	Period End	U	R	SB	L	U	R	WB	L	U	R	NB	L	U	R	EB	L	Hour	Peak
6:30	6:45	0	0	63	0	0	0	0	0	0	0	33	0	0	0	0	0	509	
6:45	7:00	0	0	54	0	0	0	0	0	0	0	61	0	0	0	0	0	601	
7:00	7:15	0	0	72	0	0	0	0	0	0	0	55	0	0	0	0	0	691	
7:15	7:30	0	0	93	0	0	0	0	0	0	0	78	0	0	0	0	0	768	
7:30	7:45	0	4	118	0	0	0	0	0	0	0	66	0	0	0	0	0	815	
7:45	8:00	0	3	127	0	0	0	0	0	0	0	72	2	0	0	0	1	871	
8:00	8:15	0	4	122	0	0	0	0	0	0	0	76	2	0	0	0	0	956	
8:15	8:30	0	2	112	0	0	0	0	0	0	0	98	2	0	0	0	4	1052	
8:30	8:45	0	4	112	0	0	3	0	1	0	2	117	2	0	0	0	3	1116	Peak
8:45	9:00	0	6	154	0	0	1	0	0	0	1	124	0	0	1	0	3		
9:00	9:15	0	2	178	0	0	0	0	0	0	0	113	5	0	0	0	2		
9:15	9:30	0	2	135	0	0	0	0	0	0	0	135	2	0	6	0	2		
14:30	14:45	0	2	123	0	0	0	0	0	0	0	111	1	0	0	0	4	1018	
14:45	15:00	0	6	143	0	0	0	0	0	0	0	108	2	0	1	0	5	1087	
15:00	15:15	0	3	112	0	0	0	0	0	0	0	139	4	0	1	0	3	1102	
15:15	15:30	0	2	97	0	0	0	0	0	0	0	139	4	0	3	0	5	1169	
15:30	15:45	0	3	165	0	0	0	0	0	0	0	128	3	0	1	0	10	1209	
15:45	16:00	0	2	126	0	0	0	0	0	0	0	144	4	0	1	0	3	1225	Peak
16:00	16:15	0	4	146	0	0	0	0	0	0	0	177	1	0	1	0	0	1225	Peak
16:15	16:30	0	2	132	0	0	1	0	0	1	0	146	2	0	2	0	4	1216	
16:30	16:45	0	4	129	1	0	0	0	1	0	0	187	1	0	1	0	2	1181	
16:45	17:00	0	5	130	0	0	0	0	0	0	1	137	1	0	1	0	5	1069	
17:00	17:15	0	1	129	0	0	0	0	0	0	0	188	1	0	0	0	1	991	
17:15	17:30	0	1	114	0	0	0	0	0	0	1	134	1	0	0	0	4	828	
17:30	17:45	0	1	70	0	0	0	0	0	0	0	138	0	0	2	0	3	720	
17:45	18:00	1	0	82	0	0	1	0	0	0	0	114	0	0	1	0	3		
18:00	18:15	0	1	63	0	0	0	0	0	0	0	92	0	0	0	0	1		
18:15	18:30	0	0	47	0	0	0	0	0	0	0	100	0	0	0	0	0		



**ANNEXURE C: SIDRA RESULTS
(4 SHEETS)**

MOVEMENT SUMMARY

▼ Site: 01 [EX AM Sydney Road / RoW (Site Folder: Existing)]

Sydney Road / Right of Way
 Existing Conditions
 AM Peak Period
 Site Category: (None)
 Give-Way (Two-Way)

Vehicle Movement Performance														
Mov ID	Turn	INPUT VOLUMES		DEMAND FLOWS		Deg. Satn v/c	Aver. Delay sec	Level of Service	95% BACK OF QUEUE		Prop. Que	Effective Stop Rate	Aver. No. Cycles	Aver. Speed km/h
		[Total veh/h]	[HV veh/h]	[Total veh/h]	[HV %]				[Veh. veh]	[Dist m]				
South: Sydney Road (S)														
2	T1	489	47	515	9.6	0.284	0.0	LOS A	0.1	0.4	0.01	0.00	0.01	49.9
3	R2	3	0	3	0.0	0.284	8.6	LOS A	0.1	0.4	0.01	0.00	0.01	49.0
Approach		492	47	518	9.6	0.284	0.1	NA	0.1	0.4	0.01	0.00	0.01	49.9
East: Right of Way (E)														
4	L2	1	0	1	0.0	0.018	10.9	LOS B	0.1	0.4	0.75	0.97	0.75	40.7
6	R2	4	0	4	0.0	0.018	18.8	LOS C	0.1	0.4	0.75	0.97	0.75	40.4
Approach		5	0	5	0.0	0.018	17.2	LOS C	0.1	0.4	0.75	0.97	0.75	40.5
North: Sydney Road (N)														
7	L2	1	0	1	0.0	0.328	4.7	LOS A	0.0	0.0	0.00	0.00	0.00	49.4
8	T1	579	41	609	7.1	0.328	0.1	LOS A	0.0	0.0	0.00	0.00	0.00	49.8
Approach		580	41	611	7.1	0.328	0.1	NA	0.0	0.0	0.00	0.00	0.00	49.8
All Vehicles		1077	88	1134	8.2	0.328	0.2	NA	0.1	0.4	0.01	0.01	0.01	49.8

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).

Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.

Delay Model: SIDRA Standard (Geometric Delay is included).

Queue Model: SIDRA Standard.

Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

MOVEMENT SUMMARY

Site: 01 [EX PM Sydney Road / RoW (Site Folder: Existing)]

Sydney Road / Right of Way
 Existing Conditions
 PM Peak Period
 Site Category: (None)
 Give-Way (Two-Way)

Vehicle Movement Performance														
Mov ID	Turn	INPUT VOLUMES		DEMAND FLOWS		Deg. Satn	Aver. Delay	Level of Service	95% BACK OF QUEUE		Prop. Que	Effective Stop Rate	Aver. No. Cycles	Aver. Speed
		[Total veh/h]	[HV] veh/h	[Total veh/h]	[HV] %				[Veh. veh]	[Dist] m				
South: Sydney Road (S)														
2	T1	654	20	688	3.1	0.361	0.0	LOS A	0.0	0.1	0.00	0.00	0.00	50.0
3	R2	1	0	1	0.0	0.361	8.5	LOS A	0.0	0.1	0.00	0.00	0.00	49.0
Approach		655	20	689	3.1	0.361	0.0	NA	0.0	0.1	0.00	0.00	0.00	50.0
East: Right of Way (E)														
4	L2	1	0	1	0.0	0.007	10.3	LOS B	0.0	0.1	0.70	0.87	0.70	41.3
6	R2	1	0	1	0.0	0.007	21.6	LOS C	0.0	0.1	0.70	0.87	0.70	41.0
Approach		2	0	2	0.0	0.007	15.9	LOS C	0.0	0.1	0.70	0.87	0.70	41.2
North: Sydney Road (N)														
7	L2	1	0	1	0.0	0.297	4.7	LOS A	0.0	0.0	0.00	0.00	0.00	49.4
8	T1	533	25	561	4.7	0.297	0.1	LOS A	0.0	0.0	0.00	0.00	0.00	49.9
Approach		534	25	562	4.7	0.297	0.1	NA	0.0	0.0	0.00	0.00	0.00	49.9
All Vehicles		1191	45	1254	3.8	0.361	0.1	NA	0.0	0.1	0.00	0.00	0.00	49.9

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).

Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.

Delay Model: SIDRA Standard (Geometric Delay is included).

Queue Model: SIDRA Standard.

Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

MOVEMENT SUMMARY

▼ Site: 01 [FU AM Sydney Road / RoW (Site Folder: Future)]

Sydney Road / Right of Way
 Future Conditions
 AM Peak Period
 Site Category: (None)
 Give-Way (Two-Way)

Vehicle Movement Performance														
Mov ID	Turn	INPUT VOLUMES		DEMAND FLOWS		Deg. Satn v/c	Aver. Delay sec	Level of Service	95% BACK OF QUEUE		Prop. Que	Effective Stop Rate	Aver. No. Cycles	Aver. Speed km/h
		[Total veh/h	HV] veh/h	[Total veh/h	HV] %				[Veh. veh	Dist] m				
South: Sydney Road (S)														
2	T1	489	47	515	9.6	0.285	0.1	LOS A	0.1	0.6	0.02	0.00	0.02	49.9
3	R2	4	0	4	0.0	0.285	8.8	LOS A	0.1	0.6	0.02	0.00	0.02	49.9
Approach		493	47	519	9.5	0.285	0.1	NA	0.1	0.6	0.02	0.00	0.02	49.9
East: Right of Way (E)														
4	L2	2	0	2	0.0	0.028	11.3	LOS B	0.1	0.6	0.74	0.97	0.74	43.4
6	R2	6	0	6	0.0	0.028	19.2	LOS C	0.1	0.6	0.74	0.97	0.74	42.6
Approach		8	0	8	0.0	0.028	17.2	LOS C	0.1	0.6	0.74	0.97	0.74	42.8
North: Sydney Road (N)														
7	L2	1	0	1	0.0	0.328	4.7	LOS A	0.0	0.0	0.00	0.00	0.00	49.4
8	T1	579	41	609	7.1	0.328	0.1	LOS A	0.0	0.0	0.00	0.00	0.00	49.8
Approach		580	41	611	7.1	0.328	0.1	NA	0.0	0.0	0.00	0.00	0.00	49.8
All Vehicles		1081	88	1138	8.1	0.328	0.3	NA	0.1	0.6	0.01	0.01	0.01	49.8

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).

Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.

Delay Model: SIDRA Standard (Geometric Delay is included).

Queue Model: SIDRA Standard.

Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

MOVEMENT SUMMARY

Site: 01 [FU PM Sydney Road / RoW (Site Folder: Future)]

Sydney Road / Right of Way
 Future Conditions
 PM Peak Period
 Site Category: (None)
 Give-Way (Two-Way)

Vehicle Movement Performance														
Mov ID	Turn	INPUT VOLUMES		DEMAND FLOWS		Deg. Satn	Aver. Delay	Level of Service	95% BACK OF QUEUE		Prop. Que	Effective Stop Rate	Aver. No. Cycles	Aver. Speed
		[Total veh/h]	[HV] veh/h	[Total veh/h]	[HV] %				[Veh. veh]	[Dist] m				
South: Sydney Road (S)														
2	T1	654	20	688	3.1	0.363	0.0	LOS A	0.1	0.4	0.01	0.00	0.01	50.0
3	R2	3	0	3	0.0	0.363	9.1	LOS A	0.1	0.4	0.01	0.00	0.01	51.5
Approach		657	20	692	3.0	0.363	0.1	NA	0.1	0.4	0.01	0.00	0.01	50.0
East: Right of Way (E)														
4	L2	1	0	1	0.0	0.012	10.4	LOS B	0.0	0.2	0.75	0.93	0.75	41.4
6	R2	2	0	2	0.0	0.012	22.0	LOS C	0.0	0.2	0.75	0.93	0.75	42.4
Approach		3	0	3	0.0	0.012	18.2	LOS C	0.0	0.2	0.75	0.93	0.75	42.0
North: Sydney Road (N)														
7	L2	3	0	3	0.0	0.298	4.7	LOS A	0.0	0.0	0.00	0.00	0.00	49.4
8	T1	533	25	561	4.7	0.298	0.1	LOS A	0.0	0.0	0.00	0.00	0.00	49.9
Approach		536	25	564	4.7	0.298	0.1	NA	0.0	0.0	0.00	0.00	0.00	49.9
All Vehicles		1196	45	1259	3.8	0.363	0.1	NA	0.1	0.4	0.01	0.01	0.01	49.9

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Site tab).

Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.

Delay Model: SIDRA Standard (Geometric Delay is included).

Queue Model: SIDRA Standard.

Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.